

Indiana Department of Transportation

County Monroe Route Vernal Pike Connector Des. No. 1702957, 1900406, 1902093

FHWA-Indiana Environmental Document CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM GENERAL PROJECT INFORMATION

Road No./County:

Vernal Pike Connector / Monroe County, Indiana

Designation Number:

1702957 (Roadway), 1900406 (Bridge), and 1902093 (Bridge)

**Project
Description/Termini:**

The extension of Sunrise Greeting Court south to the intersection of Gates Drive and the nearly constructed Profile Parkway, connecting West Vernal Pike to Gates Drive via a railroad overpass. The project will include road and bridge construction on a new alignment over CSX Railroad.

After completing this form, I conclude that this project qualifies for the following type of Categorical Exclusion (FHWA must review/approve if Level 4 CE):


	Categorical Exclusion, Level 2 – The proposed action meets the criteria for Categorical Exclusion Manual Level 2 - Table 1, CE Level Thresholds. Required Signatories: ESM (Environmental Scoping Manager)
	Categorical Exclusion, Level 3 – The proposed action meets the criteria for Categorical Exclusion Manual Level 3 - Table 1, CE Level Thresholds. Required Signatories: ESM, ES (Environmental Services Division)
X	Categorical Exclusion, Level 4 – The proposed action meets the criteria for Categorical Exclusion Manual Level 4 - Table 1, CE Level Thresholds. Required Signatories: ESM, ES, FHWA
	Environmental Assessment (EA) – EAs require a separate FONSI. Additional research and documentation is necessary to determine the effects on the environment. Required Signatories: ES, FHWA

Note: For documents prepared by or for Environmental Services Division, it is not necessary for the ESM of the district in which the project is located to release for public involvement or sign for approval.

Approval

_____ ESM Signature	_____ Date	_____ ES Signature	_____ Date
_____ FHWA Signature	_____ Date		

Release for Public Involvement

N/A	_____ Date	 ES Initials	10/23/2020 Date
ESM Initials	Date	ES Initials	Date

Certification of Public Involvement

Office of Public Involvement Date

Note: Do not approve until after Section 106 public involvement and all other environmental requirements have been satisfied.

INDOT ES/District Env. Reviewer Signature: _____ Date: _____

Name and Organization of CE/EA Preparer: Daniel J. Stevens, DLZ Indiana, LLC

This is page 1 of 33 Project Name: Vernal Pike Connector Date: October 23, 2020

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Part I - PUBLIC INVOLVEMENT

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

Does the project have a historic bridge processed under the Historic Bridges PA*?	<table border="1"><tr><td>Yes</td><td>No</td></tr><tr><td><input type="checkbox"/></td><td><input checked="" type="checkbox"/></td></tr></table>	Yes	No	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Yes	No				
<input type="checkbox"/>	<input checked="" type="checkbox"/>				
If No, then:					
Opportunity for a Public Hearing Required?	<table border="1"><tr><td><input checked="" type="checkbox"/></td><td><input type="checkbox"/></td></tr></table>	<input checked="" type="checkbox"/>	<input type="checkbox"/>		
<input checked="" type="checkbox"/>	<input type="checkbox"/>				

**A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.*

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Remarks:

Notice of Entry letters were mailed to potentially affected property owners near the project area on September 10, 2018 notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of Entry letter is included in Appendix G, pages 1 to 2.

To meet the public involvement requirements of Section 106, a legal notice of FHWA's finding of No Historic Properties Affected was published in the Bloomington Herald-Times on March 18, 2020 offering the public an opportunity to submit comment pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed 30 days later on April 18, 2020. The text of the public notice and the affidavit of publication appear in Appendix D, pages 110 to 111. No public comments were received regarding the Section 106 public notice.

The project will meet the minimum requirements described in the current *Indiana Department of Transportation (INDOT) Public Involvement Manual* which requires the project sponsor to offer the public an opportunity to submit comment and/or request a public hearing. Therefore, a legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

Public Controversy on Environmental Grounds

Will the project involve substantial controversy concerning community and/or natural resource impacts?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

At this time there is no substantial public controversy concerning impacts to the community or to natural resources.

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Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: Monroe County, Indiana INDOT District: Seymour
Local Name of the Facility: Vernal Pike Connector

Funding Source (mark all that apply): Federal ☒ State ☐ Local ☒ Other* ☐

*If other is selected, please identify the funding source: _____

PURPOSE AND NEED:

Describe the transportation problem that the project will address. The solution to the traffic problem should NOT be discussed in this section. (Refer to the CE Manual, Section IV.B.2. Purpose and Need)

Project Need

There is a gap in facility performance due to restricted access to and from SR 37/I-69. As a result of the I-69 project, access to the commercial and industrial developments in the project area (along West Vernal Pike, Sunrise Greeting Court, Enterprise Drive, North Industrial Drive, and North Packing House Road) from SR37/I-69 was cut off at West Vernal Pike and at West Whitehall Crossing Boulevard. Direct access from SR 37/I-69 to the existing commercial development, west of I-69, is now via 3rd Street on the south and SR 45/46 on the north. Access to the commercial and industrial developments along West Vernal Pike from Curry Pike is possible; however, there are three at-grade railroad crossings along Curry Pike located south of West Vernal Pike by approximately 500, 600, and 1,780 feet, respectively. These active railroad crossings cause frequent interruptions to the flow of traffic along Curry Pike and there is exposure to potential vehicle/train collisions when trains are present. Traffic coming from the northwest on Vernal Pike or from the northeast on 17th Street must negotiate the three rail crossings on Curry Pike or proceed southerly to 3rd Street east of I-69, which is difficult because there is no direct north-south road between 17th Street and 3rd Street. Additionally, prior to the development of I-69 the former SR 37 provided a single overpass over the CSX Railroad that was accessible to local north-south travelling vehicles at West Vernal Pike to the north and 3rd Street to the south. Since this bridge is no longer accessible to local north-south traffic, these vehicles must utilize the three at-grade railroad crossings on Curry Pike.

Project Purpose

The purpose of the project is to improve connectivity, improve access to and from I-69/SR 37 and existing commercial and industrial developments located near West Vernal Pike to the north and 3rd Street to the south by reducing roadway conflicts, and reduce exposure to potential vehicle/train collisions.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Monroe County Municipality: N/A

Limits of Proposed Work: The limits extend approximately 500 feet west and 100 feet east of the intersection of West Vernal Pike and Sunrise Greetings Court, then proceed south along Sunrise Greetings Court for approximately 830 feet until reaching the cul-de-sac, then extend approximately 2,055 feet to the southeast on new alignment crossing over the CSX Railroad and continuing to North Gates Drive.

Total Work Length: 0.53 Mile(s) Total Work Area: Approximately 6.55 Acre(s)

Is an Interchange Modification Study / Interchange Justification Study (IMS/IJS) required?
If yes, when did the FHWA grant a conditional approval for this project?

Yes ¹	No
	X
Date: _____	

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¹If an IMS or IJS is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IMS/IJS.

In the remarks box below, describe existing conditions, provide in detail the scope of work for the project, including the preferred alternative. Include a discussion of logical termini. Discuss any major issues for the project and how the project will improve safety or roadway deficiencies if these are issues.

Location:

The project is within Richland Township, USGS Bloomington Topographic Quadrangle, in Sections 25 and 36, Township 9 North, Range 2 West, Monroe County, Indiana. The project limits begin approximately 500 feet west and 100 feet east of the intersection of West Vernal Pike and Sunrise Greetings Court, then proceed south along Sunrise Greetings Court for approximately 830 feet until reaching the cul-de-sac, then extend approximately 2,050 feet to the southeast on new alignment crossing over the CSX Railroad and continuing to North Gates Drive. Refer to the project area location maps and project area photographs presented in Appendix B, pages 1 to 7.

Background:

The originally proposed south terminus of the Vernal Pike Connector was at Profile Parkway, a local project currently under construction with an estimated completion of December 2020. Based on comments received in response to the May 6, 2019 Section 106 early coordination request, the southern project study limits were extended approximately 550 feet south to North Gates Drive. This incorporated a portion of the locally funded Profile Parkway project into the impact assessment for the Vernal Pike Connector Project. This portion of the Profile Parkway project was included to provide the Vernal Pike Connector project with independent utility in case the locally funded Profile Parkway project was delayed or never constructed. The entire Profile Parkway project is not included since the Vernal Pike Connector does not rely on the completion of Profile Parkway to provide independent utility.

Construction of the Vernal Pike Connector will extend from south of the proposed railroad overpass to the north to West Vernal Pike. The Profile Parkway project will construct the roadway portion south of the Vernal Pike Connector to Gates Drive as shown in teal on Appendix B-3. The teal line area is included in the Vernal Pike Connector's study limits only for the purpose of assessing impacts since construction will be completed by the Profile Parkway project.

Existing Conditions:

The existing intersection of West Vernal Pike and Sunrise Greetings Court is a T-intersection controlled by a stop sign on Sunrise Greetings Court.

West Vernal Pike is functionally classified as a minor arterial roadway. It consists of two 12-foot travel lanes (eastbound and westbound), a 15-foot center turn lane, a 5-foot wide sidewalk south of the roadway and an 8-foot wide multiuse path north of the roadway with no shoulders. Curb and gutter drainage system and storm sewers are present. The posted speed limit is 35 mph. Current (2012) average daily traffic (ADT) is 4,870 vehicles per day (VPD) and design year ADT (2041) is 12,260 VPD. This increased traffic is due to potential industrial development along Profile Parkway and additional traffic from the connection to Gates Drive picking up traffic from the Whitehall Plaza/3rd Street area.

Sunrise Greeting Court is functionally classified as a local roadway. It consists of two 18-foot travel lanes (northbound and southbound), and a 5-foot wide sidewalk along the west side of the roadway. Sunrise Greeting Court widens to provide turn lanes at West Vernal Pike. Curb and gutter drainage system and storm sewers are present. There is no posted speed limit. Current (2022) ADT is unavailable for Sunrise Greeting Court. Design year ADT (2041) is 5,530 VPD. This traffic volume is due to converting Sunrise Greeting Court into the Vernal Pike Connector, additional traffic from the connection to Gates Drive picking up traffic from the Whitehall Plaza/3rd Street area, and potential industrial development along Profile Parkway.

North Gates Drive is functionally classified as a minor arterial roadway. It consists of two 18-foot travel lanes (eastbound and westbound) and 5-foot wide sidewalks both north and south of the roadway and does not include shoulders. Curb and gutter drainage system and storm sewers are present. The posted speed limit is 30 mph.

Profile Parkway is currently under construction with estimated completion by December of 2020. Profile Parkway will be classified as a major collector roadway. It will include two travel lanes and will include sidewalks along both sides of the roadway. Curb and gutter drainage system and storm sewers are proposed. The posted speed limit will be 30 mph. Design year (2041) ADT for Profile Parkway is 8,900 VPD. This predicted traffic volume is due potential industrial development along Profile Parkway, additional traffic from the connection to Gates Drive picking up traffic from the Whitehall Plaza/3rd Street area and additional traffic from the Vernal Pike Connector originating north of the railroad tracks.

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As a result of the I-69 project, access to the existing commercial and industrial developments in the project vicinity from SR-37-I-69 was cut off at West Vernal Pike and at West Whitehall Crossing Boulevard. Consequently, direct access to the existing commercial development, west of I-69, is now via 3rd Street on the south and SR 45/46 on the north. The project area roadway system's access points are deficient since the system no longer provides access from the north and east to the existing developments west of I-69.

Access to the commercial and industrial developments along West Vernal Pike from Curry Pike is possible; however, there are three at-grade railroad crossings along Curry Pike located south of West Vernal Pike by approximately 500, 600, and 1,780 feet, respectively. These active railroad crossings cause frequent interruptions to the flow of traffic along Curry Pike and there is exposure to potential vehicle/train collisions when trains are present. These three at-grade rail crossings are obstacles to ideal facility performance.

The project area contains industrial/manufacturing land along Sunrise Greetings Court. Land from Sunrise Greetings Court south to the railroad tracks is undeveloped and partially wooded land. The land immediately south of the tracks is former industrial land that is currently undeveloped and a combination of wooded land and old field. Land near the southern terminus at Gates Drive is the commercial areas referred to as Whitehall Crossing and Whitehall Plaza.

Description of Preferred Alternative:

The proposed project includes the extension of Sunrise Greeting Court south to North Gates Drive via a railroad overpass. Preliminary plans are provided in Appendix B, pages 8 to 35. The project will include road and bridge construction on a new alignment over CSX Railroad (operated by Indiana Railroad), with a total project length of approximately 0.5 miles.

Construction of the Vernal Pike Connector will extend from south of the proposed railroad overpass to the north to West Vernal Pike. The Profile Parkway project will construct the roadway portion south of the Vernal Pike Connector to Gates Drive as shown in teal on Appendix B-3. The teal line area is included in the Vernal Pike Connector's study limits only for the purpose of assessing impacts since construction will be completed by the Profile Parkway project.

The typical roadway section will consist of two 12-foot lanes, 3-foot bike lanes adjacent to the travel lanes, 8-inch concrete curb, and a 5-foot sidewalk on the west side and a 10-foot multiuse path on the east side of the proposed roadway. The design speed for the new roadway will be 35 mph. Retaining walls may be incorporated at the fill slopes of the railroad overpass bridge to optimize earth embankment cost and right-of-way. The proposed road construction will tie into the existing roadway section of Sunrise Greeting Court at the existing Cul-de-Sac, approximately 830 feet south of West Vernal Pike.

Construction of the 10-foot multiuse path will continue along the east side of Sunrise Greeting Court and will extend to West Vernal Pike. A new traffic signal will be constructed at the intersection of West Vernal Pike and Sunrise Greeting Court. The project area north of the railroad tracks will be connected to existing storm sewers. The project area south of the railroad tracks will be connected to the new Profile Parkway storm sewer system, which is currently under construction with an estimated completion by December 2020. Crosswalks and ADA compliant ramps will be included.

A parking lot connector consisting of a 50-foot-long precast reinforced concrete three-sided structure will be constructed under the Vernal Pike Connector roadway. Preliminary plan sheets of this structure are provided in Appendix B, pages 25 to 29. This structure will be located north of the railroad bridge and will provide east-west access under the Vernal Pike Connector. This is included to maintain east-west access to commercial properties' parking areas bisected by the elevated roadway.

The preliminary project's cost is estimated to be \$10,940,000, consisting of \$1,045,000 for preliminary engineering, \$1,045,000 for right of way and \$8,850,000 for construction. The project will require the acquisition of approximately 4.61 acres of land for new permanent right of way and approximately 1.11 acres for temporary right of way. The project will result in impacts to land in industrial/manufacturing use, and undeveloped land. Waterway permitting is required for impacts to wetlands.

The Preferred Alternative will satisfy the project's purpose and need to improve connectivity, improve access to and from I-69/SR 37 and existing commercial and industrial developments located near West Vernal Pike to the north and 3rd Street to the south by reducing roadway/railroad crossing conflicts along Curry Pike, and reduce exposure to potential vehicle/train collisions.

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Maintenance of Traffic:

Sunrise Greetings Court will remain open to local traffic only during construction. Access to businesses within the project area will be maintained throughout the duration of construction. The project will also require lane restrictions on Vernal Pike at the intersection with Sunrise Greetings to allow for construction of curb ramps and a new traffic signal. One through lane in each direction will be provided on Vernal Pike. Pedestrian traffic will be also be detoured during construction along West Vernal Pike and Sunrise Greeting Court.

As shown in teal on Appendix B-3, the Gates Drive intersection is currently under construction as part of the Profile Parkway project and has an estimated completion of December 2020. Gates Drive will be closed and a detour will be provided until the construction of the new intersection is completed by the Profile Parkway project.

Refer to the Maintenance of Traffic During Construction section below for further descriptions of the traffic restrictions.

Logical Termini / Independent Utility

The north project termini at West Vernal Pike and the south termini at North Gates Drive are logical termini because they are existing roadways that contain the area where facility improvement is needed. Gates Drive is utilized as the southern terminus because it is already connected to 3rd Street. Gates Drive has the capability of handling the traffic from the Vernal Pike Connector due to the existing signal at 3rd Street. West Vernal Pike is utilized as the northern terminus because it is accessible to the commercial and industrial developments along West Vernal Pike, Sunrise Greeting Court, Enterprise Drive, North Industrial Drive, North Packing House Road, and North Curry Pike. Development of the Vernal Pike Connector between these termini will satisfy the project's purpose and need to improve connectivity, improve access to and from existing commercial and industrial developments located near West Vernal Pike to the north and 3rd Street to the south by reducing roadway conflicts, and reduce exposure to potential vehicle/train collisions.

Including the portion of the Profile Parkway project as shown on Appendix B-3 in teal in this project's study limits and impact assessment enables this project to stand alone and not rely on other projects to meet the purpose and need.

OTHER ALTERNATIVES CONSIDERED:

Describe all discarded alternatives, including the Do-Nothing Alternative and an explanation of why each discarded alternative was not selected.

Do Nothing:

Under the Do Nothing or "No Build" alternative, no action would occur. With this alternative, the roadways will remain with no improvements. This alternative will not meet the stated purpose and need of the project. Therefore, this alternative has been excluded from further consideration.

Other Alternatives:

No other alternatives were considered.

The Do Nothing Alternative is not feasible, prudent or practicable because (Mark all that apply):

It would not correct existing capacity deficiencies;

It would not correct existing safety hazards;

It would not correct the existing roadway geometric deficiencies;

It would not correct existing deteriorated conditions and maintenance problems; or

It would result in serious impacts to the motoring public and general welfare of the economy.

Other (Describe): The Do Nothing alternative would not meet the project's stated purpose and need.

<input type="checkbox"/>
<input type="checkbox"/>
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<input type="checkbox"/>
<input checked="" type="checkbox"/>

ROADWAY CHARACTER:

West Vernal Pike

Functional Classification: Minor Arterial
Current ADT: 4,870 VPD (2012) Design Year ADT: 12,260 VPD (2041)

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Design Hour Volume (DHV): 1,226 Truck Percentage (%): 2%
 Designed Speed (mph): 35 Legal Speed (mph): 35

	Existing			Proposed	
Number of Lanes:	3			3	
Type of Lanes:	2 travel lanes and center turn			2 travel lanes and center turn	
Pavement Width:	39	ft.		39	ft.
Shoulder Width:	None	ft.		None	ft.
Median Width:	None	ft.		None	ft.
Sidewalk Width:	5ft/8ft	ft.		5ft/8ft	ft.

Setting: ☒ Urban ☐ Suburban ☐ Rural
 Topography: ☒ Level ☐ Rolling ☐ Hilly

Sunrise Greeting Court (Vernal Pike Connector)

Functional Classification: Local Roadway / Major Collector
 Current ADT: N/A VPD (2022) Design Year ADT: 5,530 VPD (2041)
 Design Hour Volume (DHV): 534 Truck Percentage (%): 10%
 Designed Speed (mph): 35 Legal Speed (mph): 35

	Existing			Proposed	
Number of Lanes:	2			2	
Type of Lanes:	2 travel lanes			2 travel lanes, 2 bike lanes	
Pavement Width:	36	ft.		36	ft.
Shoulder Width:	None	ft.		None	ft.
Median Width:	None	ft.		None	ft.
Sidewalk Width:	5	ft.		5ft/10ft	ft.

Setting: ☒ Urban ☐ Suburban ☐ Rural
 Topography: ☒ Level ☐ Rolling ☐ Hilly

Profile Parkway

Functional Classification: Major Collector
 Current ADT: N/A VPD (2020) Design Year ADT: 8,900 VPD (2041)
 Design Hour Volume (DHV): No Data Truck Percentage (%): No Data
 Designed Speed (mph): 35 Legal Speed (mph): 30

	Existing			Proposed	
	(To be Constructed in 2020)				
Number of Lanes:	2			2	
Type of Lanes:	2 travel lanes			2 travel lanes	
Pavement Width:	36	ft.		36	ft.
Shoulder Width:	None	ft.		None	ft.
Median Width:	None	ft.		None	ft.
Sidewalk Width:	5ft/10ft	ft.		5ft/10ft	ft.

Setting: ☒ Urban ☐ Suburban ☐ Rural
 Topography: ☒ Level ☐ Rolling ☐ Hilly

North Gates Drive

Functional Classification: Minor Arterial
 Current ADT: No Data VPD (20--)
 Design Year ADT: No Data VPD (20--)
 Design Hour Volume (DHV): No Data Truck Percentage (%): No Data
 Designed Speed (mph): 30 Legal Speed (mph): 30

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	Existing		Proposed	
Number of Lanes:	2		2	
Type of Lanes:	2 travel lanes		2 travel lanes	
Pavement Width:	36	ft.	36	ft.
Shoulder Width:	None	ft.	None	ft.
Median Width:	None	ft.	None	ft.
Sidewalk Width:	5ft/5ft	ft.	5ft/5ft	ft.

Setting: ☒ Urban ☐ Suburban ☐ Rural
 Topography: ☒ Level ☐ Rolling ☐ Hilly

If the proposed action has multiple roadways, this section should be filled out for each roadway.

DESIGN CRITERIA FOR BRIDGES:

Structure/NBI Number(s): N/A Sufficiency Rating: N/A

	Existing		Proposed	
Bridge Type:	N/A		Composite Prestressed Concrete Bulb-T Structure	
Number of Spans:	N/A		One	
Weight Restrictions:	N/A	ton	None	ton
Height Restrictions:	N/A	ft.	None	ft.
Curb to Curb Width:	N/A	ft.	34	ft.
Outside to Outside Width:	N/A	ft.	49	ft.
Shoulder Width:	N/A	ft.	5.3	ft.
Length of Channel Work:			N/A	ft.

Describe bridges and structures; provide specific location information for small structures.

Remarks:

No existing bridges or small structures are located within this project area. The project includes a new bridge over the CSX Railroad. The proposed structure will be a composite prestressed concrete bulb-t structure. The proposed structure is a single span structure. The proposed structure has a span of 137.5 feet. The proposed clear roadway width is 34 feet. The structure will carry one 12-foot-wide travel lane and a three-foot-wide bike lane in each direction. There is also a 12-foot-wide multiuse path on the east side of the structure.

Will the structure be rehabilitated or replaced as part of the project? ☐ Yes ☐ No ☒ N/A

If the proposed action has multiple bridges or small structures, this section should be filled out for each structure.

Structure/NBI Number(s): N/A Sufficiency Rating: N/A

	Existing		Proposed	
Bridge Type:	N/A		Concrete Three-Sided Structure	
Number of Spans:	N/A		One	
Weight Restrictions:	N/A	ton	None	ton
Height Restrictions:	N/A	ft.	None	ft.
Curb to Curb Width:	N/A	ft.	34	ft.
Outside to Outside Width:	N/A	ft.	49	ft.
Shoulder Width:	N/A	ft.	4	ft.
Length of Channel Work:			N/A	ft.

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Describe bridges and structures; provide specific location information for small structures.

Remarks:

No existing bridges or small structures are located within this project area. An east-west parking lot connector structure is proposed under the Vernal Pike Connector located within the north approach of the new bridge over the CSX Railroad. Preliminary plan sheets of this structure are provided in Appendix B, pages 25 to 29. This is needed to maintain east-west access to commercial properties' parking areas bisected by the elevated roadway. A proposed 50-foot-long precast reinforced concrete three-sided structure with a span of 32 feet and a rise of 18 feet will be constructed for the parking lot connector. The structure will carry one 12-foot-wide travel lane and a three-foot-wide bike lane in each direction. There is also a proposed 12-foot-wide sidewalk on the east side of the structure.

Yes No N/A

Will the structure be rehabilitated or replaced as part of the project?

☐
☐
☒

If the proposed action has multiple bridges or small structures, this section should be filled out for each structure.

MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

Is a temporary bridge proposed?

Yes

No

☐
☒

Is a temporary roadway proposed?

☐
☒

Will the project involve the use of a detour or require a ramp closure? (describe in remarks)

☒
☐

Provisions will be made for access by local traffic and so posted.

☒
☐

Provisions will be made for through-traffic dependent businesses.

☒
☐

Provisions will be made to accommodate any local special events or festivals.

☐
☒

Will the proposed MOT substantially change the environmental consequences of the action?

☐
☒

Is there substantial controversy associated with the proposed method for MOT?

☐
☒

Remarks:

The MOT for the project will keep Sunrise Greetings Court open to local traffic only. Access to businesses within the project area will be maintained throughout the duration of construction. The project will also require lane restrictions on Vernal Pike at the intersection with Sunrise Greetings to allow for construction of curb ramps and a new traffic signal. One through lane in each direction will be provided on Vernal Pike. Pedestrian traffic will be also be detoured during construction along West Vernal Pike and Sunrise Greeting Court. The Gates Drive intersection is currently under construction as part of the Profile Parkway project and has an estimated completion of December 2020. A detour is provided for Gates Drive that utilizes Alexander Drive and Jacob Drive in the Whitehall Crossing shopping area. This detour will not add any additional distance to trips along Gates Drive.

The Indiana State Festivals Association website (<http://www.indianafestivals.org>) did not identify any planned festivals within the project area. No impacts upon festivals are expected.

The closures/lane restrictions will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated and all inconveniences will cease upon project completion. Delays may occur during construction but will cease with project completion.

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ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ 1,045,000 (2019) Right-of-Way: \$ 1,045,000 (2021) Construction: \$ 8,850,000 (2022)

Anticipated Start Date of Construction: Spring 2022

Date project incorporated into STIP July 2, 2019 (Appendix H, page 4)

Is the project in an MPO Area? Yes No
☒ ☐

If yes,

Name of MPO Bloomington-Monroe County Metropolitan Planning Organization (IMPO)

Location of Project in TIP 2020-2024 TIP Project Listing (<https://bloomington.in.gov/mpo/transportation-improvement-program>, Page 19 (Appendix H, pages 1 and 2))

Date of incorporation by reference into the STIP July 2, 2019

RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0	0
Commercial	0.26	0
Agricultural	0	0
Forest	2.780*	0.205*
Wetlands	0.049*	0*
Other: Industrial	4.35	1.11
TOTAL	4.61	1.11

*Not included in the total since this land use type overlaps other categories.

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition or reacquisition, either known or suspected, and there impacts on the environmental analysis should be discussed.

Remarks: Within the project limits, the typical width of the existing West Vernal Pike right-of-way is 80 feet. The typical width of the existing Sunrise Greetings Court right-of-way is 50 feet and coincident with the edges of existing pavement. The typical width of the existing North Gates Drive right-of-way is 70 feet.

Proposed right of way width along West Vernal Pike is 80 feet. Proposed right of way width along Sunrise Greetings Court is 62 feet. Proposed right of way width along Vernal Pike Connector is 69 feet. Proposed right of way width along Profile Parkway is 90 feet. Proposed right of way width along North Gates Drive is 70 feet. Proposed right of way width will be wider at the roundabout locations at Profile Parkway/Gates Drive and Profile Parkway/Vernal Pike Connector.

The project requires approximately 4.61 acres of permanent right-of-way consisting of 0.26 acres of commercial land located south of Gates Drive and 4.35 acres of industrial land along both Sunrise Greetings Court and land south of the railroad tracks, of which 2.78 acres is forested land. The project also requires approximately 1.11 acres of temporary right-of-way from industrial land, of which 0.205 acres is forested land.

If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

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Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A – ECOLOGICAL RESOURCES

	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>No</u>
Streams, Rivers, Watercourses & Jurisdictional Ditches			
Federal Wild and Scenic Rivers			
State Natural, Scenic or Recreational Rivers			
Nationwide Rivers Inventory (NRI) listed			
Outstanding Rivers List for Indiana			
Navigable Waterways			

Remarks:

Based on a desktop review, a site visit on September 20, 2018 and October 1, 2019 by DLZ Indiana, LLC, the aerial map of the project area (Appendix B, page 3), and the water resource map in the Red Flag Investigation (RFI) report (Appendix E, page 10) there are three stream segments within the 0.5 mile search radius. No streams are present within the project area, therefore, no impacts are expected.

Early coordination letters were sent on January 25, 2019 and November 21, 2019. The Indiana Department of Environmental Management (IDEM), United States Fish and Wildlife Service (USFWS), and Indiana Department of Natural Resources Division of Fish and Wildlife (IDNR-DFW) responded on February 11, 2020 (Appendix C, pages 7 - 14); February 26, 2019 (Appendix C, pages 31 - 32) and December 5, 2019 (Appendix C, pages 33 - 36); and February 26, 2019 (Appendix C, pages 26 - 29) and December 20, 2019 (Appendix C, page 30); respectively, with recommendations to avoid or minimize impacts to natural resources. The USACE and Monroe County Surveyor did not respond.

The IDNR-DFW provided a response which indicated that approval under the regulatory programs administered by the Division of Water is not required for this project. IDNR-DFW provided recommendations regarding minimization of stream and vegetation impacts and the implementation of erosion and sedimentation control measures.

USFWS provided a response which had no objections to the project as currently proposed. USFWS also provided recommendations regarding stormwater management, in-channel construction timing, minimization of stream and vegetation impacts, and the implementation of erosion and sedimentation control measures for minimizing impacts to streams.

IDEM's electronically generated Early Coordination response included a recommendation for minimization of impacts to streams.

All applicable agency recommendations are included in the Environmental Commitments section of this CE document.

	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>No</u>
Other Surface Waters			
Reservoirs			
Lakes			
Farm Ponds			
Detention Basins			
Storm Water Management Facilities			
Other: _____			

Remarks:

Based on a desktop review, a site visit on September 20, 2018 and October 1, 2019 by DLZ Indiana, LLC, the aerial map of the project area (Appendix B, page 3), and the water resource map in the RFI report (Appendix E, page 10) there are seven other surface waters (lakes) within the 0.5 mile search radius. No other surface

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waters are present within the project area, therefore, no impacts are expected.

IDEM, USFWS, and IDNR-DFW responded on February 11, 2020 (Appendix C, pages 7 - 14); February 26, 2019 (Appendix C, pages 31 - 32) and December 5, 2019 (Appendix C, pages 33 - 36); and February 26, 2019 (Appendix C, pages 26 - 29) and December 20, 2019 (Appendix C, page 30); respectively, with recommendations to avoid or minimize impacts to natural resources. The USACE and Monroe County Surveyor did not respond.

The IDNR-DFW and USFWS provided responses did not indicate any concerns relating to project impacts upon other surface waters.

IDEM's electronically generated Early Coordination response included a recommendation for minimization of impacts to other surface waters.

All applicable agency recommendations are included in the Environmental Commitments section of this CE document.

Presence **Impacts**
Yes No

Wetlands ☐ X ☐ X ☐

Total wetland area: 0.080 acre(s) Total wetland area impacted: 0.053 acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments
A	PFO	0.027	0	Wetland A is located east of the study limits and north of the railroad tracks and will be avoided.
B	PFO	0.049	0.049	Wetland B is located south of the tracks and is unavoidable due to the required bridge approach footprint and alignment.
C	PEM	0.019	0.004	Wetland C is located along a manmade drainage swale located north of the railroad tracks and west of the alignment.

Wetlands (Mark all that apply)	Documentation	ES Approval Dates
Wetland Determination	<input type="checkbox"/> X	<input type="text" value="N/A (LPA Project)"/>
Wetland Delineation	<input type="checkbox"/> X	<input type="text" value="N/A (LPA Project)"/>
USACE Isolated Waters Determination	<input type="checkbox"/>	<input type="text"/>
Mitigation Plan	<input type="checkbox"/>	<input type="text"/>

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

Substantial adverse impacts to adjacent homes, business or other improved properties;
 Substantially increased project costs;
 Unique engineering, traffic, maintenance, or safety problems;
 Substantial adverse social, economic, or environmental impacts, or
 The project not meeting the identified needs.

☐ X
☐
☐ X
☐
☐

Measures to avoid, minimize, and mitigate wetland impacts need to be discussed in the remarks box.

Remarks: Based on a review of the National Wetlands Inventory (NWI) online mapper (<https://www.fws.gov/wetlands/data/Mapper.html>), a site visit on September 20, 2018 and October 1, 2019 by DLZ Indiana, LLC, the USGS topographic map (Appendix B page 2), and the RFI report (Appendix E page 10) there are 3 NWI wetlands and one NWI line feature located within the 0.5 mile search radius. There are three wetlands present within or adjacent to the project area.

A Waters of the U.S. Determination / Wetland Delineation Report was completed for the project on November

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15, 2019. Please refer to Appendix F, page 1 for the Waters of the U.S. Determination / Wetland Delineation Report. It was determined that three wetland features (Wetland A, Wetland B, and Wetland C) were identified in the study limits. The USACE makes all final determinations regarding jurisdiction.

Wetland A is a 0.027-acre forested wetland located in the wooded area east of the parking lot/drive near the southern end of Sunrise Greetings Court. The quality of Wetland A is considered poor due to its small size and eroded nature. Wetland A is located east of the project construction limits and impacts to this feature will be avoided.

Wetland B is a 0.049-acre forested wetland located south of the railroad tracks. The quality of Wetland B is considered poor due to its small size, lack of plant diversity, and apparent manmade nature. The project will impact the entire limit of Wetland B. Impacts to this feature are unavoidable due to the required bridge alignment and the size of the bridge approach footprint.

Wetland C is a 0.019-acre emergent wetland located within a manmade drainage swale in the lawn area south of the parking lot to the north of the railroad tracks. Wetland C is situated near the west edge of the roadway alignment. The boundary of Wetland C extends west beyond the study limits of this report. The quality of Wetland C is considered poor due to its small size, manmade nature, and since it is frequently mowed. The project will impact approximately 0.004-acre of Wetland C. Impacts to this feature are unavoidable due to its proximity to the alignment. This alignment must be maintained to minimize impacts to industrial properties located both east and west of the alignment.

A USACE Section 404 Permit will likely be required. In the event a Section 404 Permit is required, a Section 401 Water Quality Certification must also be obtained from the Indiana Department of Environmental Management (IDEM) Office of Water Quality. Wetland mitigation is not expected to be required since the impacts are less than the 0.10-acre threshold.

IDEM, USFWS, and IDNR-DFW responded on February 11, 2020 (Appendix C, pages 7 - 14); February 26, 2019 (Appendix C, pages 31 - 32) and December 5, 2019 (Appendix C, pages 33 - 36); and February 26, 2019 (Appendix C, pages 26 - 29) and December 20, 2019 (Appendix C, page 30); respectively, with recommendations to avoid or minimize impacts to natural resources. The USACE did not respond.

The IDNR-DFW response included a recommendation for minimization of impacts to wetlands.

USFWS provided a response which did not indicate any concerns regarding project impacts upon wetlands.

IDEM's electronically generated Early Coordination response noted that projects affecting wetlands must obtain the appropriate permits and that projects involving more than 0.5 acre of wetland impacts should undergo additional coordination with IDEM.

All applicable agency recommendations are included in the Environmental Commitments section of this CE document.

	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>No</u>
Terrestrial Habitat	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Unique or High Quality Habitat	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Use the remarks box to identify each type of habitat and the acres impacted (i.e. forested, grassland, farmland, lawn, etc).

Remarks:

Based on a desktop review, a site visit on September 20, 2018 and October 1, 2019 by a DLZ Indiana, LLC, the aerial map of the project area (Appendix B, page 3), there are terrestrial habitats consisting of forest, old field (former industrial/commercial) and lawn (commercial/industrial properties). Not including the existing roadways, approximately 3.92 acres of such terrestrial habitat area will be affected which consist of approximately 2.48 acres of forest, 0.49 acre of old field, and 0.95 acre of lawn.

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Dominant tree species in the forested habitat are sassafras (*Sassafras albidum*), black cherry (*Prunus serotina*), tulip tree (*Liriodendron tulipifera*), black walnut (*Juglans nigra*), eastern red cedar (*Juniperus virginiana*), and honeysuckle (*Lonicera tatarica*). Dominant old field species include tall goldenrod (*Solidago altissima*), blackberry (*Rubus allegheniensis*), multiflora rose (*Rosa multiflora*), honeysuckle (*Lonicera tatarica*), staghorn sumac (*Rhus typhina*), sassafras (*Sassafras albidum*), eastern red cedar (*Juniperus virginiana*), white oldfield American aster (*Symphyotrichum pilosum*), and smooth brome (*Bromus inermis*). The lawn areas include typical turfgrass species such as tall fescue (*Schedonorus arundinaceus*), bluegrass (*Poa pratensis*), white clover (*Trifolium repens*), dandelion (*Taraxacum officinale*), English plantain (*Plantago lanceolata*), and crabgrass (*Digitaria sanguinalis*).

Avoidance alternatives are not practical since the project provides a connection between the termini while avoiding the relocation of existing business. In addition, the bridge location and alignment geometry requirements do not allow for lateral alignment shifting to avoid these terrestrial habitats. The requirement for habitat mitigation is not anticipated.

USFWS and IDNR-DFW responded on February 26, 2019 (Appendix C, pages 31 - 32) and December 5, 2019 (Appendix C, pages 33 - 36); and February 26, 2019 (Appendix C, pages 26 - 29) and December 20, 2019 (Appendix C, page 30); respectively, with recommendations to avoid or minimize impacts to natural resources.

USFWS provided recommendations regarding limiting the amount of clearing of trees or understory to minimize terrestrial habitat impacts. USFWS indicated that tree clearing should be conducted between November 15 and March 30. If this measure is implemented, USFWS concurs that the proposed project is not likely to adversely affect the Indiana bat or the northern long-eared bat. No trees will be cleared during the restricted period.

The IDNR-DFW response included a recommendation for minimization of forest and vegetation impacts, potential mitigation measures, protection and revegetation of disturbed areas, and seasonal tree cutting restrictions in order to minimize terrestrial habitat impacts. Habitat avoidance alternatives are not practical for the reasons noted above in this section.

All applicable agency recommendations are included in the Environmental Commitments section of this CE document.

If there are high incidences of animal movements observed in the project area, or if bridges and other areas appear to be the sole corridor for animal movement, consideration of utilizing wildlife crossings should be taken.

Karst

Is the proposed project located within or adjacent to the potential Karst Area of Indiana?

If yes, are karst features located within or adjacent to the footprint of the proposed project?

If yes, will the project impact any of these karst features?

Yes

No

X
X
X

Use the remarks box to identify any karst features within the project area. (Karst investigation must comply with the Karst MOU, dated October 13, 1993).

Remarks:

Based on a desktop review, the project is located inside the designated karst region of Indiana as outlined in the October 13, 1993 Memorandum of Understanding (MOU). According to the topo map of the project area (Appendix B, page 2), and the RFI report (Appendix E, pages 3 and 4) there are karst features identified within or adjacent to the project area.

A karst study was prepared by Hydrogeology, Inc. (Appendix J) that identified one potential sinkhole in the survey area. This feature is a surface depression approximately 15 feet in diameter and approximately one foot deep. The report noted that the depression was filled with water and it was not possible to determine the exact nature of the depression. Per the karst study, it is possible that the depression is manmade.

Additional karst survey was conducted of this feature (VP-1) by Hydrogeology, Inc on June 8, 2020 using Electrical Resistivity Imaging (ERI) (Appendix J, page 13). The specific karst feature is not shown in the appendices because it is a sensitive resource. This investigation concluded that due to the lack of surface

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openings this feature would not be suitable habitat for cave organisms. The ERI profiles conducted adjacent to VP-1 showed no evidence of a large karst void in bedrock or preferential pathways from the surface depression through the soil. Additionally, ERI profiles showed no evidence that the depression was a large sinkhole that had been filled. The ERI profiles showed potential karst development near the top of bedrock, likely the result of epikarst, which is highly weathered carbonate bedrock immediately beneath the surface or beneath the soil. As currently designed, the surface depression will be under Sunrise Greeting Court. It is recommended that the surface depression be further investigated by excavation during construction to rule out the possibility it could pose a structural concern for the roadway.

INDOT's Ecology and Waterway Permitting Office (INDOT EWPO) was consulted and indicated that the karst investigations conducted are sufficient. INDOT EWPO indicated that no additional coordination with agencies is warranted based on the results of the investigations. INDOT EPWO did not have concerns over groundwater contamination if all exposed sinkholes are properly capped.

The identified potential sinkhole is located south of the railroad tracks within the proposed southern bridge approach and will be impacted by the project. Avoidance alternatives are not practicable due to the potential sinkhole location and the size of the bridge approach footprint required. The alignment cannot be shifted east or west far enough to avoid this feature without creating undesirable bridge and roadway geometry.

A firm commitment has been made that either prior to or during construction, exploratory excavation utilizing best practices will be conducted to determine if the potential sinkhole is a confirmed sinkhole. If the sinkhole is confirmed it will be capped in accordance with the MOU.

Although Monroe County not a MOU signatory, the project will follow the terms of the MOU as closely as possible.

Monroe County has a zoning ordinance (Monroe County Code Chapter 829) regarding karst and sinkhole development standards. Since this project is a Monroe County Highway project it is exempt from compliance with this ordinance (Appendix C, page 43). However, the County will address the potential sinkhole consistent with the MOU.

In the early coordination response, the USFWS (Appendix C, page 31) indicated that the project is in the karst area of Indiana and in or adjacent to an area that was noted during I-69 studies to have a sinking stream system. USFWS noted that if any karst features are encountered, a karst survey should be conducted, with mitigation measures as necessary, in accordance with our 1993 Memorandum of Understanding. USFWS suggested that coordination be performed with IDEM for groundwater and karst issues. As noted above, karst investigations have been conducted.

As suggested by USFWS, correspondence was submitted to IDEM Geological Services. Correspondence received from IDEM Geological Services (Appendix C, Page 15) indicated that it appears the new road will extend across a portion of the Former ABB plant. Given the known environmental conditions and current property restrictions, IDEM recommended coordination with the IDEM Federal Programs Section and the USEPA Remedial Project Manager, for further technical evaluation and guidance prior to proceeding with the new road. As suggested, correspondence was sent to IDEM and USEPA on April 9, 2019. No response was received from either agency. However, as noted under below under Section H – Hazardous Materials and Regulated Substances, coordination is ongoing with IDEM and USEPA regarding the Former ABB project for the adjacent Profile Parkway Project.

In the early coordination response, the Indiana Geological Survey (IGS) did indicate that karst features may exist in the project area (Appendix C, page 17).

The IGS Environmental Assessment Report noted the following in the general vicinity of the project:

- Geological Hazards: Potential Karst, High liquefaction potential
- Mineral Resources: bedrock resource - Low potential, and sand and gravel resource - low potential
- Active or abandoned mineral resources extraction sites: None documented in the area

Response from IGS has been communicated with the designer on February 11, 2020.

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Threatened or Endangered Species

Within the known range of any federal species
Any critical habitat identified within project area
Federal species found in project area (based upon informal consultation)
State species found in project area (based upon consultation with IDNR)

Presence

X
X

Impacts

Yes

X

No

X

Is Section 7 formal consultation required for this action?

Yes

--

No

X

Remarks:

Based on a desktop review and the RFI report (Appendix E), completed by DLZ Indiana, LLC on December 23, 2019, the IDNR Monroe County Endangered, Threatened and Rare (ETR) Species List has been checked and is included in (Appendix E, pages 13 - 16). The highlighted species on the list reflect the federal and state identified ETR species located within the county. According to the IDNR-DFW early coordination response letters dated February 26, 2019 and December 20, 2019 (Appendix C, page 26 and Appendix C, page 30), the Natural Heritage Program's Database has been checked and to date no plant or animal species listed as state or federally threatened, endangered, or rare have been reported in the project vicinity.

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, pages 37 - 42). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (NLEB) (*Myotis septentrionalis*). No additional species were found within or adjacent to the project area other than the Indiana bat and northern long-eared bat.

Based on tree clearing extending beyond 300 feet from any roadway, this project does not qualify for the *Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB)*. Early coordination letters were sent on January 25, 2019 and November 21, 2019. USFWS responded on February 26, 2019 (Appendix C, pages 31 - 32) and December 5, 2019 (Appendix C, pages 33 - 36). USFWS indicated there are numerous records of both Indiana bat and NLEB in Monroe County. In addition, the project is within a 10-mile buffer of designated Critical Habitat for the Indiana bat. There is suitable summer habitat for both of these species present throughout the area surrounding the project site, including wooded areas within the project boundary. The project is also near several Indiana bat and northern long-eared bat hibernacula, as well as within designated Indiana bat critical habitat. Per the USFWS, the project will not eliminate enough habitat to affect these species, but to avoid incidental take from removal of an occupied roost tree, tree clearing should be conducted between Nov. 15-March 30. If this measure is implemented the USFWS concurs that the proposed project is not likely to adversely affect the Indiana bat or the northern long-eared bat.

The project includes a firm commitment that tree clearing will only be conducted between November 15 and March 30.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

SECTION B – OTHER RESOURCES

Drinking Water Resources

Wellhead Protection Area
Public Water System(s)
Residential Well(s)
Source Water Protection Area(s)
Sole Source Aquifer (SSA)

Presence

X
X

Impacts

Yes

No

X
X

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If a SSA is present, answer the following:

	Yes	No
Is the Project in the St. Joseph Aquifer System?	<input type="checkbox"/>	<input type="checkbox"/>
Is the FHWA/EPA SSA MOU Applicable?	<input type="checkbox"/>	<input type="checkbox"/>
Initial Groundwater Assessment Required?	<input type="checkbox"/>	<input type="checkbox"/>
Detailed Groundwater Assessment Required?	<input type="checkbox"/>	<input type="checkbox"/>

Remarks:

The project is located in Monroe County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project. Therefore a detailed groundwater assessment is not needed and no impacts are expected.

The Indiana Department of Environmental Management's Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on November 25, 2019 by DLZ. This project is not located within a Wellhead Protection Area or Source Water Area. No impacts are expected.

The Indiana Department of Natural Resources Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on November 25, 2019 by DLZ. A well is located south of west Vernal Pike and east of Sunrise Greeting Court. This feature will not be affected because it is not located in the proposed right-of-way. Therefore, no impacts are expected. Should it be determined during the right-of-way phase that this well is affected, a cost to cure will likely be included in the appraisal to restore the well.

Based on a desktop review of the INDOT MS4 website (<https://entapps.indot.in.gov/MS4/>) by DLZ on November 25, 2019, and the RFI report; this project is located in an Urban Area Boundary (UAB) location. An early coordination letter was sent on November 25, 2019, to the Monroe County MS4 Coordinator. The MS4 coordinator did not respond within the 30-day time frame.

Based on a desktop review, a site visit on September 20, 2018 and October 1, 2019 by DLZ, and the aerial map of the project area (Appendix B, page 3), this project is located where there is a public water system. The public water system will not be affected because the water line has been identified and will be avoided by construction activities. An early coordination letter was sent on April 30, 2019 to City of Bloomington Utilities and no utility conflicts were identified. Utility coordination is ongoing and will continue through Stage 3 plan development. There are no other public/private water utilities in the project area; therefore, the project is not anticipated to result in impacts upon public drinking water supplies.

Flood Plains

Longitudinal Encroachment
Transverse Encroachment
Project located within a regulated floodplain
Homes located in floodplain within 1000' up/downstream from project

Presence	Impacts	
	Yes	No
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Discuss impacts according to classification system described in the "Procedural Manual for Preparing Environmental Studies".

Remarks:

The Indiana Department of Natural Resources Indiana Floodway Information Portal website (<http://dnrmmaps.dnr.in.gov/appsphp/fdms/>) was accessed on November 25, 2019 by DLZ. This project is not located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F, page 65 A). Therefore, it does not fall within the guidelines for the implementation of 23 CFR 650, 23 CFR 771, and 44 CFR. No impacts are expected.

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Farmland	Presence	Impacts	
		Yes	No
Agricultural Lands	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Prime Farmland (per NRCS)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Total Points (from Section VII of CPA-106/AD-1006* N/A

**If 160 or greater, see CE Manual for guidance.*

See CE Manual for guidance to determine which NRCS form is appropriate for your project.

Remarks:

Based on a desktop review, a site visit on September 20, 2018 and October 1, 2019 by DLZ, the aerial map of the project area (Appendix B, page 3), there is no land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) within or adjacent to the project area. The requirements of the FPPA do not apply to this project; therefore, no impacts are expected. An early coordination letter was sent on January 25, 2019 and November 21, 2019, to Natural Resources Conservation Services (NRCS) and in a reply dated March 5, 2019 and November 27, 2019 (Appendix C, page 24 and Appendix C, page 25) the NRCS indicated the project will not cause a conversion of prime farmland.

SECTION C – CULTURAL RESOURCES

	Category	Type	INDOT Approval Dates	N/A
Minor Projects PA Clearance	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Results of Research

Eligible and/or Listed Resource Present

Archaeology	<input type="checkbox"/>
NRHP Buildings/Site(s)	<input type="checkbox"/>
NRHP District(s)	<input type="checkbox"/>
NRHP Bridge(s)	<input type="checkbox"/>

Project Effect

No Historic Properties Affected ☒ No Adverse Effect ☐ Adverse Effect ☐

Documentation Prepared

Documentation (mark all that apply)

		ES/FHWA Approval Date(s)	SHPO Approval Date(s)
Historic Properties Short Report	<input type="checkbox"/>		
Historic Property Report	<input checked="" type="checkbox"/>	5/3/2019, 1/2/2020 (addendum)	7/12/2019, 2/6/2020 (addendum)
Archaeological Records Check/ Review	<input type="checkbox"/>		
Archaeological Phase Ia Survey Report	<input checked="" type="checkbox"/>	5/3/2019, 1/2/2020 (addendum)	7/12/2019, 2/6/2020 (addendum)
Archaeological Phase Ic Survey Report	<input type="checkbox"/>		
Archaeological Phase II Investigation Report	<input type="checkbox"/>		
Archaeological Phase III Data Recovery	<input type="checkbox"/>		
APE, Eligibility and Effect Determination	<input checked="" type="checkbox"/>	3/12/2020	4/13/2020
800.11 Documentation	<input checked="" type="checkbox"/>	3/12/2020	4/13/2020

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MOA Signature Dates (List all signatories)

Memorandum of Agreement (MOA)

Describe all efforts to document cultural resources, including a detailed summary of the Section 106 process, using the categories outlined in the remarks box. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of paper(s) and the comment period deadline. Likewise include any further Section 106 work which must be completed at a later date, such as mitigation or deep trenching.

Remarks:

Area of Potential Effect (APE):

The APE delineated for this project extends approximately 500 feet to the west and 1,000 feet to the east from the project end points along West Vernal Pike, then approximately 700 feet on each side of Sunrise Greeting Court until the beginning of new alignment, at which point the APE expands to roughly a 1350 feet radius around the location of the proposed elevated bridge crossing the railway, and continuing south to a point approximately 350 feet south of Jonathan Drive acting as the APE's southern boundary (Appendix D, page 10).

Coordination with Consulting Parties:

The Indiana Department of Natural Resources State Historic Preservation Officer (SHPO) was automatically included as a consulting party. On May 9, 2019, potentially interested parties were invited to participate as consulting parties in the Section 106 review for the project. The invited parties are indicated in the table below.

Invited Section 106 Consulting Parties	Status
Monroe County Board of Commissioners	No Response
Monroe County Highway Superintendent	No Response
Monroe County Historian	No Response
Monroe County History Center	No Response
Bloomington/Monroe County Metropolitan Planning Organization	Accepted
Indiana Landmarks Central Regional Office	Accepted
Bloomington Restorations, Inc.	No Response
Downtown Bloomington Commission	No Response
Monroe County Historic Preservation Board of Review	Accepted
Duncan Campbell Preservation Development, Inc.	No Response
City of Bloomington Historic Preservation Program Manager	No Response
City of Bloomington Street Department	No Response
Delaware Tribe of Indians, Oklahoma	No Response
Eastern Shawnee Tribe of Oklahoma	No Response
Miami Tribe of Oklahoma	Accepted
Peoria Tribe of Indians of Oklahoma	No Response
Pokagon Band of Potawatomi Indians	No Response

Initial Consultation

An early coordination letter was sent to the SHPO on May 6, 2019, and the other consulting parties received it via email on May 9, 2019 (Appendix D, pages 79 – 89). The early coordination letter noted the availability of the Historic Property Report (HPR), which contains information regarding the project's APE, and the identification of historic properties within the APE.

In a letter dated May 9, 2019 (Appendix D, page 90), the Bloomington/Monroe County MPO indicated that the proposed project is wholly consistent with the Bloomington-Monroe County 2040 Metropolitan Transportation Plan, the FY 2018-2021 Transportation Improvement Program, and the Draft FY 2020-2024 Improvement Program. No concerns regarding historic resources were raised by the MPO.

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In a letter dated May 29, 2019 (Appendix D, page 91), the Monroe County Historic Board of Review concurred with the report's recommendations, and it agreed to a finding of no adverse effect to historic properties.

In a letter dated June 6, 2019 (Appendix D, page 92), the Indiana Landmarks Central Regional Office concurred that there are no properties listed in or eligible for listing in the NRHP within the proposed APE for the project. Indiana Landmarks also recommended considering enlarging the APE to including Profile Parkway, a locally funded project. In response to this comment, the APE was enlarged to include the segment of Profile Parkway required to connect the Vernal Pike Connector to North Gates Drive. This was done to provide the Vernal Pike Connector project with independent utility in case the locally funded Profile Parkway project was delayed or never constructed. The entire Profile Project is not included since the Vernal Pike Connector does not rely on the completion of Profile Parkway to provide independent utility.

The SHPO provided comments dated July 12, 2019 (Appendix B, pages 93 - 94) indicating that the APE in the HPR appears to be of appropriate size to encompass the geographic area in which direct and indirect effects of this project could occur. The SHPO also agreed with the HPR's recommendations that there do not appear to be any individual, above-ground properties or historic districts within the APE that would be eligible for inclusion in the NRHP. The SHPO also concurred with the recommendations of the archeological report and agreed that no further archaeological investigations appear necessary at the proposed project area.

No responses were received from the other consulting parties regarding the initial consultation.

Additional Consultation

Additional consultation was performed due to changes in the project limits. Based on comments received in response to the May 6, 2019 early coordination, the southern project study limits were extended approximately 550 feet south to North Gates Drive. This incorporated a portion of the locally funded Profile Parkway project into the impact assessment for the Vernal Pike Connector Project. This portion of the Profile Parkway project was included to provide the Vernal Pike Connector project with independent utility in case the locally funded Profile Parkway project was delayed or never constructed. All consulting parties were sent the additional consultation letter. A hard copy of the additional consultation letter was sent to the SHPO on January 2, 2019, and the other consulting parties received it via email on January 2, 2019 (Appendix D, pages 96 - 106). The additional consultation letter noted the availability of the Addendum HPR, which contains information regarding the project's APE, and the identification of historic properties within the APE.

In a letter dated January 6, 2020 (Appendix D, page 107), the Miami Tribe indicated they have no objection to the project and are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to the project site. However, as this project is within the aboriginal homelands of the Miami Tribe, if any human remains or Native American cultural items falling under the Native American Graves Protection and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of this project, the Miami Tribe requests immediate consultation with the entity of jurisdiction for the location of discovery.

The SHPO provided comments dated February 6, 2020 (Appendix D, pages 108 - 109) indicating that the revised APE in the HPR appears to be of appropriate size to encompass the geographic area in which direct and indirect effects of this project could occur. The SHPO also agreed with the HPR addendum's recommendations that there do not appear to be any individual, above-ground properties or historic districts within the APE that would be eligible for inclusion in the NRHP. The SHPO also concurred with the recommendations of the addendum archeological report and agreed that no further archaeological investigations appear necessary at the proposed project area.

Archaeology:

NS Services, LLC completed an archaeological records check and Phase 1a Archaeological Field Reconnaissance (2018) of the project area. The Phase 1a report notes that the archaeological reconnaissance located no archaeological sites within the project area and recommended that the project be allowed to proceed as planned. The report was approved by INDOT's Cultural Resources Office (CRO) on May 3, 2018. The report was provided to the SHPO for review and comment on May 6, 2019. In a letter dated July 12, 2019, the SHPO concurred with the archaeological report's recommendations (Appendix D, pages 74 - 75).

An Addendum Phase 1a Field Reconnaissance (NS Services, LLC, 2019) was prepared for the expansion of the study limits to include a portion of the locally funded Profile Parkway Project. As a result of these efforts,

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no sites were recommended as eligible for listing in the NRHP in the expanded project area and no further work is recommended. The report was provided to the SHPO for review and comment on January 2, 2020. In a letter dated February 6, 2020, the SHPO concurred with the archaeological report's recommendations (Appendix D, pages 76 - 77).

Historic Properties:

The Historic Property Report (HPR) prepared for this project (H&H Associates, LLC, 2018) noted that no properties listed in the NRHP are located within the APE, and recommended that no properties eligible for listing in the NRHP are present within the APE. INDOT's CRO released the HPR for distribution to all consulting parties on May 3, 2019. DLZ provided the HPR to the SHPO for review and comment on May 6, 2019 and the other consulting parties received it via email on May 9, 2019. In a letter dated July 12, 2019, the SHPO concurred with the HPR's recommendations regarding the APE and the NRHP eligibility of properties within the APE for listing in the NRHP (Appendix D, pages 74 - 75).

An Addendum HPR (H&H, 2019) was prepared for the expansion of the APE to include a portion of the locally funded Profile Parkway Project. As a result of the historic property identification and evaluation efforts, no above-ground resources are recommended as eligible for listing in the NRHP in the expanded project area. In a letter dated February 6, 2020, the SHPO concurred with the Addendum HPR's recommendations regarding the APE and the NRHP eligibility of properties within the APE for listing in the NRHP (Appendix D, pages 76 - 77).

Documentation, Findings:

Because no historic properties were identified within the APE, a finding of No Historic Properties Affected was made. On March 12, 2020, INDOT's CRO, acting on FHWA-IN's behalf, issued a finding of No Historic Properties Affected for this undertaking. The SHPO concurred with the finding in a letter dated April 13, 2020 (Appendix D, pages 112 - 113).

Public Involvement:

Views of the public pertaining to the INDOT CRO/Federal Highway Administration (FHWA) Finding of No Historic Properties Affected were sought through publication of a legal notice in the Bloomington Herald-Times newspaper (March 18, 2020). Comments were requested within 30 days of the publication date. No comments were received. The affidavit for publication of the legal notice is presented as Appendix D, pages 110 - 111.

The Section 106 process has been completed and FHWA's Section 106 responsibilities have been fulfilled.

SECTION D – SECTION 4(f) RESOURCES / SECTION 6(f) RESOURCES

Section 4(f) Involvement (mark all that apply)

Parks & Other Recreational Land

Publicly owned park
Publicly owned recreation area
Other (school, state/national forest, bikeway, etc.)

Presence

Use

Yes	No

Evaluations Prepared

Programmatic Section 4(f)*
"De minimis" Impact*
Individual Section 4(f)

FHWA Approval date

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Wildlife & Waterfowl Refuges	<u>Presence</u>	<u>Use</u>	
		Yes	No
National Wildlife Refuge	<input type="text"/>	<input type="text"/>	<input type="text"/>
National Natural Landmark	<input type="text"/>	<input type="text"/>	<input type="text"/>
State Wildlife Area	<input type="text"/>	<input type="text"/>	<input type="text"/>
State Nature Preserve	<input type="text"/>	<input type="text"/>	<input type="text"/>

	<u>Evaluations Prepared</u>	<u>FHWA Approval date</u>
Programmatic Section 4(f)*	<input type="text"/>	<input type="text"/>
"De minimis" Impact*	<input type="text"/>	
Individual Section 4(f)	<input type="text"/>	

Historic Properties	<u>Presence</u>	<u>Use</u>	
		Yes	No
Sites eligible and/or listed on the NRHP	<input type="text"/>	<input type="text"/>	<input type="text"/>

	<u>Evaluations Prepared</u>	<u>FHWA Approval date</u>
Programmatic Section 4(f)*	<input type="text"/>	<input type="text"/>
"De minimis" Impact*	<input type="text"/>	
Individual Section 4(f)	<input type="text"/>	

**FHWA approval of the environmental document also serves as approval of any Section 4f Programmatic and/or De minimis evaluation(s) discussed below.*

Discuss Programmatic Section 4(f) and "de minimis" Section 4(f) impacts in the remarks box below. Individual Section 4(f) documentation must be separate Draft and Final documents. For further discussions on Programmatic, "de minimis" and Individual Section 4(f) evaluations please refer to the "Procedural Manual for the Preparation of Environmental Studies". Discuss proposed alternatives that satisfy the requirements of Section 4(f).

Remarks:

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, a site visit on September 20, 2018 by DLZ, the aerial map of the project area (Appendix B, page 3), and the RFI report (Appendix E, page 2) there are five potential 4(f) resources located within the 0.5 mile search radius. There are three within or adjacent to the project area. These consist of the Vernal Pike Sidepath (located along the north side of West Vernal Pike) and two segments of the planned Stinesville/Elettsville Greenway (both shown in the current location of the existing CSX Railroad tracks).

Per the Monroe County Transportation Alternatives Plan (February 21, 2018), sidepaths and greenways are considered transportation alternatives. Therefore, these facilities are not Section 4(f) resources per 23 CFR 774.13(f)(4) since they are part of the local transportation system and function primarily for transportation. The plan identifies a sidepath as a transportation alternative developed as a road improvement opportunity within the road right-of-way for continuity of the multimodal network. Additionally, the plan states that both recreation and transportation uses can be accommodated within greenway corridors. As the network becomes more complete, recreational uses often transition to become transportation uses. Whenever feasible, the Monroe County Transportation Alternatives Plan recommends incorporating greenways as a transportation alternative.

The project will connect the new sidewalks to the Vernal Pike Sidepath via new ADA compliant curb ramps. No negative impacts to the Vernal Pike Sidepath will occur.

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In addition, the project will span the existing CSX railroad in the location of the planned segments of the Stinesville/Ellettsville Greenway and will not impact the future development of these facilities. Therefore, no impacts upon these planned facilities are expected.

Section 6(f) Involvement

Presence

Use

Yes

No

Section 6(f) Property

☐☐☐

Discuss proposed alternatives that satisfy the requirements of Section 6(f). Discuss any Section 6(f) involvement.

Remarks:

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of 6(f) properties on the Land and Water Conservation Fund (LWCF) website at <https://www.lwcfcoalition.com/tools> revealed a total of 19 properties in Monroe County (Appendix L, page 1). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources as a result of this project.

SECTION E – Air Quality

Air Quality

Conformity Status of the Project

Is the project in an air quality non-attainment or maintenance area?

Yes

No

☐☒

If YES, then:

Is the project in the most current MPO TIP?

☐☐

Is the project exempt from conformity?

☐☐

If the project is NOT exempt from conformity, then:

Is the project in the Transportation Plan (TP)?

☐☐

Is a hot spot analysis required (CO/PM)?

☐☐

Level of MSAT Analysis required?

Level 1a ☐ Level 1b ☒ Level 2 ☐ Level 3 ☐ Level 4 ☐ Level 5 ☐

Remarks:

This project is included in the Fiscal Year (FY) 2020-2024 Bloomington/Monroe County Metropolitan Planning Organization Transportation Improvement Program (MPO TIP) and Statewide Transportation Improvement Program (STIP) (Appendix H, pages 1 - 2).

This project is located in Monroe County, which is currently in attainment for all criteria pollutants according to the IDEM Nonattainment Status for Indiana Counties website (https://www.in.gov/idem/airquality/files/nonattainment_areas_map.pdf). Therefore, the conformity procedures of 40 CFR Part 93 do not apply.

The purpose of this project is to provide a connection from West Vernal Pike to Profile Parkway/Gates Drive by constructing a new roadway and bridge over CSX Railroad. This project has been determined to generate minimal air quality impacts for Clean Air Act criteria pollutants and has not been linked with any special mobile source air toxic (MSAT) concerns. As such, this project will not result in changes in traffic

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volumes, vehicle mix, basic project location, or any other factor that would cause a meaningful increase in MSAT impacts of the project from that of the no-build alternative.

Moreover, Environmental Protection Agency (EPA) regulations for vehicle engines and fuels will cause overall MSAT emissions to decline significantly over the next several decades. Based on regulations now in effect, an analysis of national trends with EPA's MOVES2014 model forecasts a combined reduction of over 90 percent in the total annual emissions rate for the priority MSAT from 2010 to 2050 while vehicle-miles of travel are projected to increase by over 45 percent. This will both reduce the background level of MSAT as well as the possibility of even minor MSAT emissions from this project.

SECTION F - NOISE

Noise

Yes

No

Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?

☒

☐

No Yes/ Date

ES Review of Noise Analysis

Yes / March 19, 2020

Remarks:

A Traffic Noise Study was prepared by DLZ (Appendix I, pages 1 to 47). INDOT Environmental Services Division (ES) reviewed the noise study for the above-referenced project and found it to be technically sufficient on March 19, 2020 (Appendix I, page 48).

Based on the studies completed to date, DLZ has identified no impacted receptors. As a result, noise abatement was not evaluated. This noise analysis was based on preliminary design criteria. A reevaluation of the noise analysis will occur during final design. If during final design it has been determined that conditions have changed and noise impacts are identified, noise abatement will be evaluated at that time as to whether it is feasible and reasonable.

SECTION G - COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

Will the proposed action comply with the local/regional development patterns for the area?

Yes

No

☒

☐

Will the proposed action result in substantial impacts to community cohesion?

☐

☒

Will the proposed action result in substantial impacts to local tax base or property values?

☐

☒

Will construction activities impact community events (festivals, fairs, etc.)?

☐

☒

Does the community have an approved transition plan?

☐

☒

If No, are steps being made to advance the community's transition plan?

☒

☐

Does the project comply with the transition plan? (explain in the remarks box)

☒

☐

Remarks:

No negative regional, community or neighborhood impacts are anticipated. Access to all properties within the project area will be maintained during construction. Therefore, no impacts are expected.

The Indiana State Festivals Association website (<http://www.indianafestivals.org>) did not identify any planned festivals within the project area. No impacts upon festivals are expected.

With regard to compliance with the draft transition plan (Monroe County, 2019), the project involves construction of ADA compliant pedestrian facilities and is therefore in compliance.

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Indirect and Cumulative Impacts

Will the proposed action result in substantial indirect or cumulative impacts?

Yes

☐

No

☒

Remarks:

Indirect impacts are effects which are caused by the action and are later in time or farther removed in distance, but are still reasonably foreseeable. Indirect effects may include growth inducing effects and other effects related to induced changes in the pattern of land use, population density, or growth rate. Cumulative impacts affect the environment which result from the incremental impact of the action when added to other past, present, and reasonably foreseeable future actions regardless of what agency or person undertakes such actions.

The project is improving access to an area where access was limited as a result of the I-69 project. Land in the project vicinity is already being redeveloped and it is possible that due to the improved/restored access to this area that new development could result in the project vicinity and along the new roadway. However, the project is not expected to result in substantial foreseeable indirect or cumulative impacts. The project is not expected to affect future changes in land use in the area.

Monroe County Urbanizing Area Plan (October 2015) describes land use in the project area as employment and mixed-use. The Vernal Pike Connector project is compatible with these land uses. On November 19, 2019, the Monroe County Plan Commission approved the ABB Inc. Major Subdivision Preliminary Plat for the approximately 69.1 acre parcel located at 300 N. Curry Pike. This parcel contains the Profile Parkway (under construction) and maintains frontage along N. Curry Pike and W. Jonathon Drive. This parcel was divided into 15 lots of record for general industry. The Vernal Pike Connector will tie into Profile Parkway and will provide improved access to the new redevelopment area.

Public Facilities & Services

Will the proposed action result in substantial impacts on health and educational facilities, public and private utilities, emergency services, religious institutions, airports, public transportation or pedestrian and bicycle facilities? *Discuss how the maintenance of traffic will affect public facilities and services.*

Yes

☐

No

☒

Remarks:

Based on a desktop review, a site visit on September 20, 2018 and October 1, 2019 by DLZ Indiana, LLC, the aerial map of the project area (Appendix B, page 3) and the RFI report (Appendix E, page 2), there are five private pipelines, three private railroad segments and four trails located within the 0.5 mile of the project. The railroads and trails are within the project area. The RFI identified an airport approximately 2.41 miles from the project area.

There is Vectren gas pipeline easement located along the south side of the Indiana Railroad Company tracks that will be spanned by the project. Utility coordination has been conducted and is ongoing with Vectren. Therefore, no impacts to the pipeline easement are expected.

The railroads identified in the RFI within the project area are operated by the Indiana Railroad Company. The project includes a new bridge over these railroads. Railroad coordination has been conducted and is ongoing with Indiana Railroad Company. Therefore, no impacts to the railroad are expected.

The trails identified in the RFI are the Monroe County Vernal Pike Sidepath and two planned Monroe County segments of the Stinesville/Ellettsville Greenway. The project will connect new sidewalks to the Vernal Pike Sidepath via new ADA compliant curb ramps. No negative impacts to the Vernal Pike Sidepath will occur. In addition, the project will span the existing CSX railroad in the location of the planned segments of the Stinesville/Ellettsville Greenway and will not impact the future development of these facilities. Therefore, no impacts are expected to the planned trails.

In a February 4, 2019 Early Coordination response (Appendix C, page 21), the INDOT Department of Aviation indicated that the Monroe County Airport is located approximately 2.1 nautical miles northeast of the proposed project corridor. An Indiana Tall Structure permit would not be required unless the project involves the construction of a temporary (e.g., crane) or permanent structure that penetrates a 100:1 slope from the nearest point of the Monroe County Airport runway. The project will not involve a permanent or temporary

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structure that penetrates 100:1 slope. Therefore, no impacts upon the Monroe County Airport or airspace are expected.

In a November 26, 2019 Early Coordination response (Appendix C, page 22), the INDOT Department of Aviation indicated that if any object, obstruction, or equipment will exceed 75 feet in height, further coordination is required with the INDOT Department of Aviation because of the close proximity of the Monroe County Airport. The project will not involve any object, obstruction, or equipment that will exceed 75 feet in height. The height of the bridge is approximately 38 feet. Therefore, no impacts upon the Monroe County Airport or airspace are expected.

Early coordination letters were sent to the Monroe County Community School Corporation and Monroe County Sheriff's Office on January 25, 2019 and November 21, 2019. These agencies did not respond to the early coordination letters.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

Will the project result in adversely high or disproportionate impacts to EJ populations?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. Per the current INDOT Categorical Exclusion Manual, an Environmental Justice (EJ) Analysis is required for any project that has two or more relocations or 0.5 acre of additional permanent right-of-way. The project will not require any relocations; however, it will require greater than 0.5 acre of additional permanent right-of-way. Therefore, an EJ Analysis is required.

Potential EJ impacts are detected by locating minority and low-income populations relative to a reference population to determine if populations of EJ concern exists and whether there could be disproportionately high and adverse impacts to them. The reference population may be a county, city or town and is called the community of comparison (COC). In this project, the COC is Monroe County, Indiana. The project is located just outside of the Bloomington city limits and within Monroe County. The community that overlaps the project area is called the affected community (AC). In this project, the AC is Census Tract 13.01. An AC has a population of concern for EJ if the population is more than 50% minority or low-income or if the low-income or minority population is 125% of the COC. Data from the 2013 to 2017 American Community Survey (ACS) 5-year estimates was obtained from the US Census Bureau Website <https://factfinder.census.gov/> on November 21, 2019 by DLZ. The data collected for minority and low-income populations within the AC are summarized in the below table.

Table: Minority and Low-Income Data (2013-2017 American Community Survey 5-Year Estimates)		
	COC - (Monroe County, Indiana)	AC-1 - (Census Tract 13.01, Monroe County, Indiana)
Percent Minority	15.87%	6.99%
125% of COC	19.83%	AC < 125% COC
EJ Population of Concern		No
Percent Low-Income	24.73%	7.04%
125% of COC	30.91%	AC < 125% COC
EJ Population of Concern		No

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AC-1, Census Tract 13.01, Monroe County, Indiana has a percent minority of 6.99% which is below 50% and is below the 125% COC threshold. Therefore, AC-1 does not contain minority populations of EJ concern.

AC-1, Census Tract 13.01, Monroe County, Indiana has a percent low-income of 7.04% which is below 50% and is below the 125% COC threshold. Therefore, AC-1 does not contain low-income populations of EJ concern.

Conclusion

Since no EJ populations were identified, the project will have no adversely high and disproportionate impact. No further environmental justice analysis is warranted.

The census data sheets, map, and calculations can be found in Appendix K.

Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses or farms?

Is a Business Information Survey (BIS) required?

Is a Conceptual Stage Relocation Study (CSRS) required?

Has utility relocation coordination been initiated for this project?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

Number of relocations: Residences: 0 Businesses: 0 Farms: 0 Other: N/A

If a BIS or CSRS is required, discuss the results in the remarks box.

Remarks: No relocations of people, businesses, or farms will take place as a result of this project.

SECTION H – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Hazardous Materials & Regulated Substances (Mark all that apply)

Red Flag Investigation

Phase I Environmental Site Assessment (Phase I ESA)

Phase II Environmental Site Assessment (Phase II ESA)

Design/Specifications for Remediation required?

Documentation

<input checked="" type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>

No Yes/Date

ES Review of Investigations		Yes/December 23, 2019
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Include a summary of findings for each investigation.

Remarks:

Based on a review of GIS and available public records, a RFI was completed on December 23, 2019 by DLZ Indiana, LLC (Appendix E, page 1). The RFI did not cover the expanded study area. Re-coordination was conducted with INDOT Site Assessment and Management (SAM) regarding the expanded study area and ABB Soil Management Plan. INDOT SAM provided guidance to use the original RFI and had no additional requirements based on the Soil Management Plan. A subsequent review was conducted for the expanded area, as noted below. Seven RCRA Generator/TSD sites, two underground storage tank (UST) sites, three Leaking Underground Storage (LUST) sites, one Brownfield site, one Institutional Control site and seven NPDES Facilities are located within 0.5 mile of the project area, and two RCRA Generator/TSD sites, are located within the project area; however, no hazmat sites were identified in or within 0.5 mile of the project area that will impact the project.

The nearest RCRA Generator/TSD site is located at the north end of the project area. The nearest UST site 0.38 mile from the project area. The nearest LUST site is 0.33 mile from the project area. The nearest Institutional Control site is 0.38 mile from the project area. The nearest NPDES site is 0.07 mile from the

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project area. The nearest Brownfield site is 0.38 mile from the project area. No impacts are expected. Further investigation for sites identified in the RFI for hazardous material concerns is not required at this time.

Additional Project Study Area

Additional study area was added to include an approximately 550-foot portion of the locally funded Profile Parkway project to provide a connection the Vernal Pike Connector to North Gates Drive. Red Flag data was reviewed for the additional study area. Three additional UST sites, one additional LUST site, one additional NPDES site and one additional NPDES Pipe location were within the expanded 0.5 mile radius of the project area. The result of the expansion of the project study limits results in one of the previously identified RCRA Generator/TSD sites being closer to the project. This site is the Circle-Proscio, Inc. located at 401 N. Gates Drive near the proposed intersection of North Gates Drive and Profile Parkway. The expanded project limits will require right of way from the front lawn of this facility. A review of the IDEM Virtual File Cabinet indicated the facility is classified as a Small Quantity Generator and has had no violations reported. Therefore, no hazardous material impacts are anticipated.

Early coordination correspondence from IDEM Geological Services (Appendix C, Page 15) received on February 28, 2019 indicated that it appears the new road will extend across a portion of the Former ABB plant. Given the known environmental conditions and current property restrictions, IDEM Geological Services suggested coordination with the IDEM Federal Programs Section and the USEPA Remedial Project Manager for further technical evaluation and guidance prior to proceeding with the new road. As suggested, correspondence was sent to IDEM and USEPA on April 9, 2019. No response was received from either agency. However, as noted below, coordination is ongoing with IDEM and USEPA regarding the Former ABB project for the adjacent Profile Parkway Project.

Former ABB Site

The former ABB, Inc. manufacturing plant (ABB Site) was not shown in the RFI data. The ABB Site is located at 300 North Curry Pike in Bloomington, Indiana (ABB Site) that was part of a cleanup site subject to an Administrative Settlement Agreement and Consent EPA-V-W-08-C-890 (AOC) issued by the United States Environmental Protection Agency (EPA) and approved by IDEM. The ABB Site is subject to an Environmental Protection Easement and Declaration of Restrictive Covenants (ERC) made by ABB on February 2, 2012. The ERC has placed restrictions on the use of the ABB Site and the handling and disposal of contaminated soils that were left in place within the designated Affected Areas. The Vernal Pike Connector is not located within any of the designated Affected Areas. The nearest the project limits approach any of the designated affected Areas is approximately 350 feet. Portions of the locally funded Profile Parkway project beyond the Vernal Pike Connector study limits are located within designated Affected Areas. The ERC also states that a Soil Management Plan must be prepared and implemented prior to any excavation, or other similar disturbances of soils within the Affected Areas. As part of the locally funded Profile Parkway Project and in response to the requirements stated in the ERC, DLZ prepared a Soil Management Plan on behalf of the Monroe County Board of Commissioners that satisfies the requirements of the ERC and all local, state, and federal regulations. The Soil Management Plan was submitted to IDEM for review on July 21, 2019 and was approved by IDEM on August 27, 2019. Excerpts from the Soil Management Plan and the IDEM approval letter are include in Appendix E, pages 17 - 126 and Appendix E, page 51, respectively. The project includes a firm commitment to comply with the Soil Management Plan.

Any potential hazardous material concerns associated with potential sinkholes will be addressed by all of the exposed sinkholes being properly capped.

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SECTION I – PERMITS CHECKLIST

Permits (mark all that apply)

Likely Required

Army Corps of Engineers (404/Section 10 Permit)

Individual Permit (IP)	<input type="checkbox"/>
Nationwide Permit (NWP)	<input type="checkbox"/>
Regional General Permit (RGP)	<input checked="" type="checkbox"/>
Pre-Construction Notification (PCN)	<input type="checkbox"/>
Other	<input type="checkbox"/>
Wetland Mitigation required	<input type="checkbox"/>
Stream Mitigation required	<input type="checkbox"/>

IDEM

Section 401 WQC	<input checked="" type="checkbox"/>
Isolated Wetlands determination	<input type="checkbox"/>
Rule 5	<input checked="" type="checkbox"/>
Other	<input type="checkbox"/>
Wetland Mitigation required	<input type="checkbox"/>
Stream Mitigation required	<input type="checkbox"/>

IDNR

Construction in a Floodway	<input type="checkbox"/>
Navigable Waterway Permit	<input type="checkbox"/>
Lake Preservation Permit	<input type="checkbox"/>
Other	<input type="checkbox"/>
Mitigation Required	<input type="checkbox"/>

US Coast Guard Section 9 Bridge Permit

Others (Please discuss in the remarks box below)

<input type="checkbox"/>

Remarks:

The proposed work is anticipated to disturb more than one acre; therefore, it will be necessary for IDEM to issue a Rule 5 permit.

The project will result in impacts to jurisdictional surface water and will require a Section 401 Water Quality Certification (IDEM) and Section 404/Section 10 permit (USACE).

If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

Indiana Department of Transportation

County Monroe Route Vernal Pike Connector Des. No. 1702957, 1900406, 1902093

SECTION J- ENVIRONMENTAL COMMITMENTS

The following information should be provided below: List all commitments, name of agency/organization requesting the commitment(s), and indicating which are firm and which are for further consideration. The commitments should be numbered.

Remarks:

Firm:

1. If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD and INDOT District)
2. It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
3. To avoid incidental take of Indiana bat or NLEB from removal of an occupied roost tree, tree clearing will only be conducted between Nov. 15-March 30. (USFWS and IDNR)
4. Either prior to or during construction, exploratory excavation utilizing best practices will be conducted to determine if the potential sinkhole is a confirmed sinkhole. If the sinkhole is confirmed coordination will occur with INDOT EWPO. (INDOT ESD)
5. Any construction activities performed on the ABB Site will comply with the Soil Management Plan. (INDOT ESD)

For Further Consideration:

1. Avoid all work within the inundated part of the stream channel (in perennial streams and larger intermittent streams) during the fish spawning season (April 1 through June 30), except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season. No equipment should be operated below Ordinary High Water Mark during this time unless the machinery is within the caissons or on the cofferdams. (USFWS)
2. Restrict below low-water work to placement of piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap. (USFWS)
3. Construct new structures with a widened span and benches on one or both sides to provide for wildlife crossing, if practical. The crossing should be above normal high water, relatively flat and with natural substrate suitable for use by a wide variety of wildlife. (USFWS)
4. If riprap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat. (USFWS)
5. Post DO NOT DISTURB signs at the construction zone boundaries and do not clear trees or understory vegetation outside the boundaries. (USFWS)
6. USFWS recommends using pollutant-trapping technology such as storm drain inserts, etc. to reduce runoff of urban pollutants directly to the stream system. (USFWS)
7. Culverts should span the active stream channel, should be either embedded or a 3-sided or open-arch culvert, and be installed where practicable on an essentially flat slope. When an open-bottomed culvert or arch is used in a stream, which has a good natural bottom substrate, such as gravel, cobbles and boulders, the existing substrate should be left undisturbed beneath the culvert to provide natural habitat for the aquatic community. (USFWS)

Indiana Department of Transportation

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8. Minimize the extent of hard armor (riprap) in bank stabilization by using bioengineering techniques whenever possible. If rip rap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat. (USFWS)
9. Evaluate wildlife crossings under bridge/culverts projects in appropriate situations. Suitable crossings include flat areas below bridge abutments with suitable ground cover, high water shelves in culverts, amphibian tunnels and diversion fencing. (USFWS)
10. Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to non-wetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10" dbh or greater (5:1 mitigation based on the number of large trees). (IDNR)
11. The Bloomington area is generally characterized by underlying karst geology; therefore, the IDNR recommends implementation of the 1993 INDOT/IDNR/IDEM/USFWS Karst Memorandum of Understanding during all phases of the project (http://www.in.gov/indot/files/38_karst.pdf). (IDNR)
12. Should any karst features be located within the construction limits or that may receive drainage from the construction, a karst assessment should be conducted by a qualified geologist and a determination made as to whether or not the karst feature/sinkhole is active. If a karst assessment is not done, any sinkhole that construction runoff may drain to should be assumed to be active. To protect active sinkholes (or those not assessed), the most protective erosion control methods should be implemented to avoid potentially impacting sensitive karst ecosystems (such as runoff containment and filtering prior to discharge). (IDNR)
13. Construction should be avoided within 25' of the topmost closed contour of any active karst features. Where construction within the closed contours of a karst feature is unavoidable, runoff must be filtered prior to discharge. (IDNR)
14. Lighting: The International Dark-Sky Association (IDA) states that, to minimize the negative impacts of artificial lighting on wildlife, "lighting should only be on when needed, only light the area that needs it, be no brighter than necessary, minimize blue light emissions, [and] be fully shielded (pointing downward)". The Division of Fish and Wildlife strongly encourages visiting the IDA's website to learn more about selecting lighting fixtures that minimize the harmful effects of lighting on humans and wildlife: <http://darksky.org/lighting/lighting-basics/>. (IDNR)
15. Place the trail in or adjacent to existing right-of-ways where possible to minimize significant impacts to natural resource habitat. Also, utilize previously disturbed or degraded areas. Align the trail along or near existing man-made edges or areas that have the potential to be restored or enhanced by trail construction (i.e. railroad corridors), rather than routing the trail through previously undisturbed areas. (IDNR)
16. When designing or constructing a trail, disturb as narrow an area as possible to help minimize negative impacts. Where significant impacts to fish, wildlife or botanical resources are likely due to the trail's width, reduce the width to help avoid those impacts. ADA accessibility standards allow departures from the standards under certain conditions, including substantial harm to natural features, habitat, or vegetation (see <http://www.access-board.gov/attachments/article/1500/outdoor-rule.pdf>, Accessibility Guidelines for Outdoor Developed Areas). (IDNR)
17. Do not focus only on the direct impact of the trail's width; also consider the trail's impact to the surrounding habitat. Trails can fragment larger habitat areas and reduce the overall usefulness of the site to fish, wildlife, or botanical resources (1 large habitat block is better than 2 small habitat blocks), Trails can cause significant impacts to forested areas, riparian forested corridors along creeks and rivers, and wetland areas. They also may cause sediment and erosion issues or introduce human disturbance into fairly isolated areas containing wildlife habitat. (IDNR)

Indiana Department of Transportation

County Monroe Route Vernal Pike Connector Des. No. 1702957, 1900406, 1902093

18. Avoid elements identified in the Natural Heritage Database; trails may negatively affect species that require specific natural conditions (vegetation, light levels, moisture, etc.) that are altered as a result of trail construction. Rare and high quality habitats, and wildlife habitats that possess high wildlife abundance and diversity, should be avoided by placing the trail around the habitat and screening it from the trail and trail users with a buffer of native vegetation or another method as discussed below. Wetlands and karst features are but two examples of areas to avoid. (IDNR)
19. Screen wildlife habitat from the trail corridor. Vegetation, topography, and fences can help reduce the impact of noise and line of sight disturbances of trail users on wildlife. Walls can create wildlife movement barriers and potential impacts must be considered. Native grass buffers (2 to 3 feet tall) are recommended along the edge of trails near habitat such as wetlands. (IDNR)
20. Lighting should only be used when absolutely necessary. Lighting in forested areas and along creeks, streams, and rivers should be the lowest intensity feasible and shielded to cast light on the path and not diffused into the surroundings to avoid disturbing wildlife circadian rhythms and disorienting night-migrating birds. (IDNR)
21. Shoulders should be constructed using unconsolidated materials where possible. In some situations, solid shoulders are necessary. In those cases, shoulders should be constructed using porous concrete. (IDNR)
22. Post "Do Not Mow or Spray" signs along the right-of-way. (IDNR)

Indiana Department of Transportation

County Monroe Route Vernal Pike Connector Des. No. 1702957, 1900406, 1902093

SECTION K- EARLY COORDINATION

Please list the date coordination was sent and all agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received. INDOT and FHWA are automatically considered early coordination participants and should only be listed if a response is received.

Remarks:

Agency/Party	Response Date(s)
Federal and State Natural Resource/Regulatory Agencies	
Sent 1/25/2019 and 11/21/2019	
USDA – Natural Resources Conservation Service	3/5/2019 and 11/27/2019
Indiana Geological Survey	2/11/2020
IDNR Division of Fish and Wildlife	2/26/2019 and 12/20/2019
INDOT – Aviation Section; Inter-Modal Transportation Division	2/4/2019 and 11/26/2020
National Park Service	No Response
IDEM – Automatic Website Coordination	2/11/2020
IDEM – Office of Land Quality	2/28/2019
IDEM – Jessica Fliss	No Response
USEPA – Thomas Alcamo	No Response
US Department of Housing and Urban Development	No Response
INDOT – Office of Communications	1/29/2019
INDOT – Environmental Scoping Manager – Seymour District	No Response
Louisville District, Corps of Engineers	No Response
USFWS	2/26/2019 and 12/5/2019
USFWS Rangewide Programmatic Consultation – Species List	2/12/2020
Forest Supervisor, Hoosier National Forest, US Forest Service	No Response
Local and County Agencies – Sent 1/25/2019 and 11/21/2019	
Bloomington/Monroe County Metropolitan Planning Organization	2/14/2019
Monroe County MS4 Assistant	No Response
Monroe County Drainage Engineer	No Response
Monroe County Surveyor	No Response
Monroe County Sheriff	No Response
Northern Monroe Co. Fire Protection Territory	No Response
Monroe County Community School Corporation	No Response
Monroe County Commissioners	No Response
Monroe County Parks and Recreation	No Response

FHWA and INDOT are automatic consulting parties, thus they were sent copies of the Early Coordination materials on 1/25/2019 and 11/21/2019. Early Coordination and other correspondence are presented in Appendix C.

Vernal Pike Connector
From West Vernal Pike to Profile Parkway
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Monroe County, Indiana

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APPENDIX A

INDOT Supporting Documentation



Vernal Pike Connector
From West Vernal Pike to Profile Parkway
Des. No.: 1702957 & 1900406
Monroe County, Indiana

Appendix A

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	"No Historic Properties Affected"	"No Adverse Effect"	-	"Adverse Effect" Or Historic Bridge involvement ²
Stream Impacts	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	Individual 404 Permit
Wetland Impacts	No adverse impacts to wetlands	< 0.1 acre	-	< 1 acre	≥ 1 acre
Right-of-way³	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)	"No Effect", "Not likely to Adversely Affect" (Without AMMs ⁴ or with AMMs required for all projects ⁵)	"Not likely to Adversely Affect" (With any other AMMs)	-	"Likely to Adversely Affect"	Project does not fall under Species Specific Programmatic
Threatened/Endangered Species (Any other species)	Falls within guidelines of USFWS 2013 Interim Policy	"No Effect", "Not likely to Adversely Affect"	-	-	"Likely to Adversely Affect"
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁶
Sole Source Aquifer	Detailed Assessment Not Required	-	-	-	Detailed Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Coastal Zone Consistency	Consistent	-	-	-	Not Consistent
National Wild and Scenic River	Not Present	-	-	-	Present
New Alignment	None	-	-	-	Any
Section 4(f) Impacts	None	-	-	-	Any
Section 6(f) Impacts	None	-	-	-	Any
Added Through Lane	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Coast Guard Permit	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ⁷
Approval Level <ul style="list-style-type: none"> District Env. Supervisor Env. Services Division FHWA 	Concurrence by INDOT District Environmental or Environmental Services	Yes	Yes	Yes Yes	Yes Yes Yes

¹Coordinate with INDOT Environmental Services. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

²Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³Permanent and/or temporary right-of-way.

⁴AMMs = Avoidance and Mitigation Measures.

⁵AMMs determined by the IPAC decision key to be needed that are listed in the USFWS User's Guide for the Range-wide Programmatic Consultation for Indiana bat and Northern long-eared bat as "required for all projects".

⁶Potential for causing a disproportionately high and adverse impact.

⁷Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

*Substantial public or agency controversy may require a higher-level NEPA document.

APPENDIX B

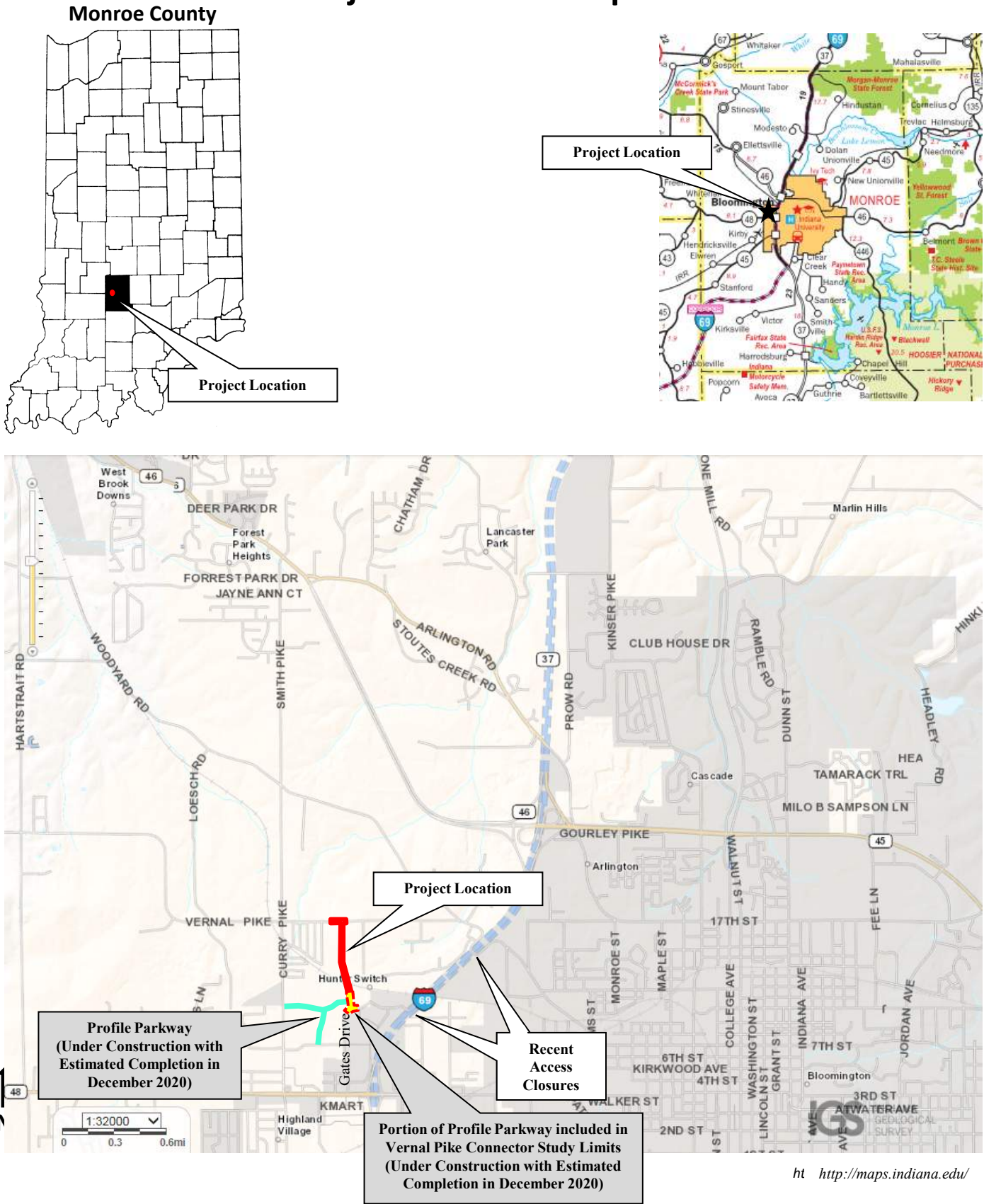
Graphics



Vernal Pike Connector
From West Vernal Pike to Profile Parkway
Des. No.: 1702957 & 1900406
Monroe County, Indiana

Appendix B

Project Location Graphics

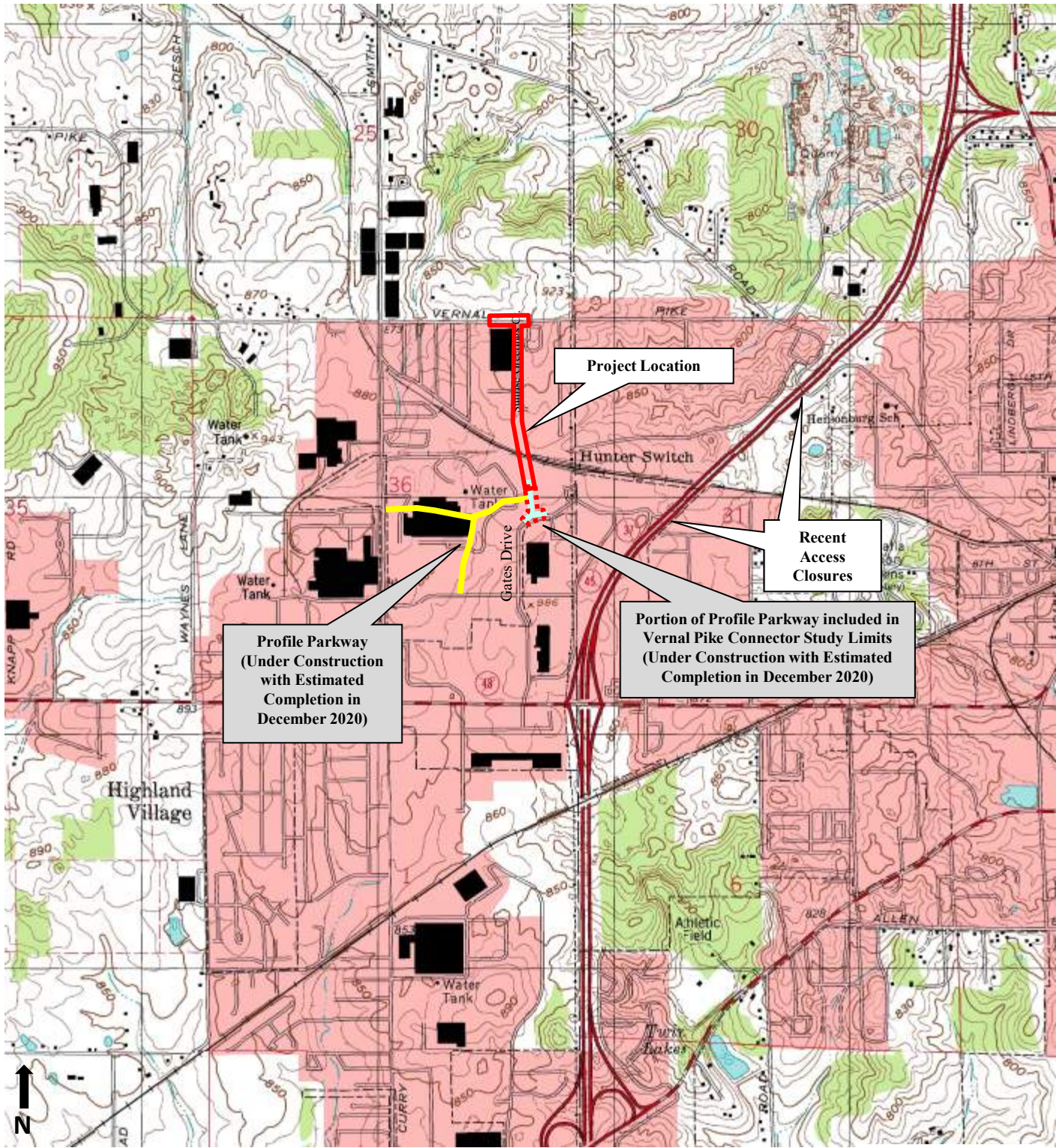


Vernal Pike Connector
From West Vernal Pike to Profile Parkway
Des. No.: 1702957 & 1900406
Monroe County, Indiana

Scale: NTS

Appendix B-1

USGS Quadrangle Map



USGS Bloomington Quadrangle Map

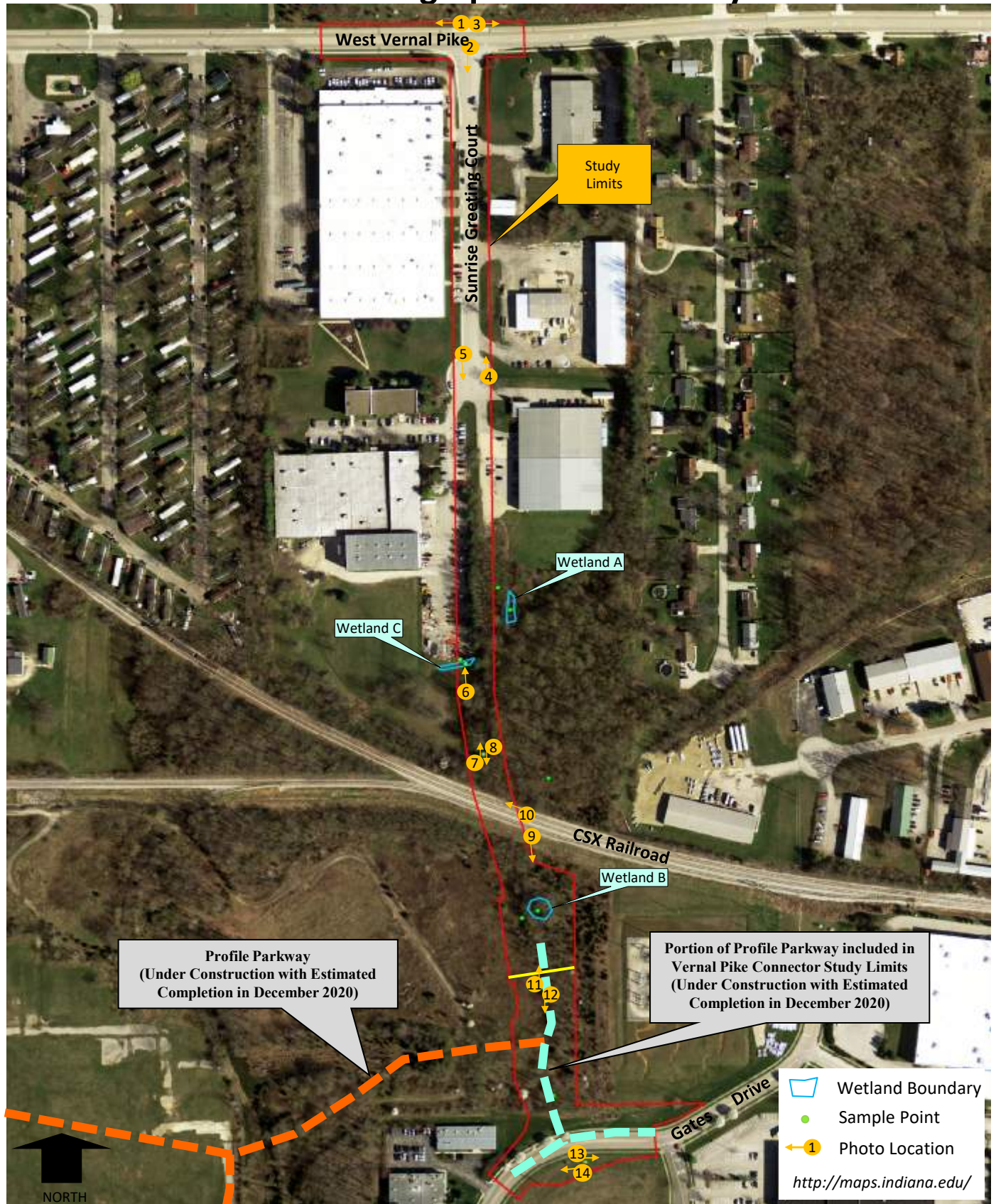


Vernal Pike Connector
From West Vernal Pike to Profile Parkway
Des. No.: 1702957 & 1900406
Monroe County, Indiana

Scale: 1"=2000'

Appendix B-2

Aerial Photograph and Photo Key



Vernal Pike Connector
From West Vernal Pike to Profile Parkway
Des. No.: 1702957 & 1900406
Monroe County, Indiana

Scale: 1"=333'

Appendix B-3

Photographs - 9/20/2018



Photo 1: Looking west along West Vernal Pike



Photo 2: Looking south along Sunrise Greetings Court



Photo 3: Looking east along West Vernal Pike



Photo 4: Looking north along Sunrise Greetings Court
from cul-de-sac



WETLAND DELINEATION
Vernal Pike Connector
From West Vernal Pike to Profile Parkway
Des. No.: 1702957 and 1900406
Monroe County, Indiana

Site Photos

Appendix B-4

Photographs - 9/20/2018



Photo 5: Looking north along Sunrise Greetings Court from cul-de-sac



Photo 6: Looking north along drive and wooded area.



Photo 7: Looking north from north of CSX RR at SP-9



Photo 8: Looking south from north of CSX RR at SP-9

	<p>WETLAND DELINEATION Vernal Pike Connector From West Vernal Pike to Profile Parkway Des. No.: 1702957 and 1900406 Monroe County, Indiana</p>	Scale: NTS
		Appendix B-5

Photographs - 9/20/2018



Photo 9: Looking south across CSX Railroad tracks



Photo 10: Looking west along the CSX Railroad tracks



Photo 11: Looking north from north of
N. Gates Drive at SP-11



Photo 12: Looking south from north of
N. Gates Drive at SP-11



WETLAND DELINEATION
Vernal Pike Connector
From West Vernal Pike to Profile Parkway
Des. No.: 1702957 and 1900406
Monroe County, Indiana

Scale: NTS

Appendix B-6

Photographs - 10/1/2019



Photo 13: View east from south of
North Gates Drive at SP-10



Photo 14: View west from south of
North Gates Drive at SP-10



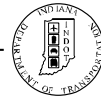
WETLAND DELINEATION
Vernal Pike Connector
From West Vernal Pike to Profile Parkway
Des. No.: 1702957 and 1900406
Monroe County, Indiana

Scale: NTS

Appendix B-7

PROJECT	DESIGNATION
1702957	1702957
CONTRACT	BRIDGE FILE
R-0090	---

INDIANA DEPARTMENT OF TRANSPORTATION



MONROE COUNTY
BOARD OF COMMISSIONERS

ROAD PLANS

STAGE 2 PLANS
APRIL 2020

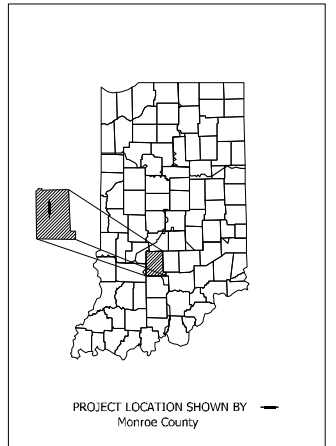
ROUTE: SUNRISE GREETING COURT

PROJECT NO. 1702957 P.E.
PROJECT NO. 1702957 R/W
PROJECT NO. 1702957 CONST.

Extension of Sunrise Greeting Court from existing cul-de-sac to Profile Parkway
Approx. 0.5 miles west of I-69 in Section 36, T-9-N, R-2-W, Richland Township, Monroe County, Indiana

TRAFFIC DATA	
A.A.D.T. (2041)	5,530 V.P.D.
D.V.M. (2041)	534 V.P.D.
DIRECTIONAL DISTRIBUTION	50%
TRUCKS	10% A.A.D.T.
DESIGN DATA	
DESIGN SPEED	35 M.P.H.
PROJECT DESIGN CRITERIA	NEW CONSTRUCTION (NON-HIGHWAY)
FUNCTIONAL CLASSIFICATION	MAJOR COLLECTOR
RURAL/URBAN	URBAN
TERRAIN	LEVEL
ACCESS CONTROL	NONE

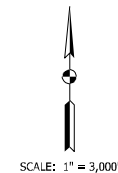
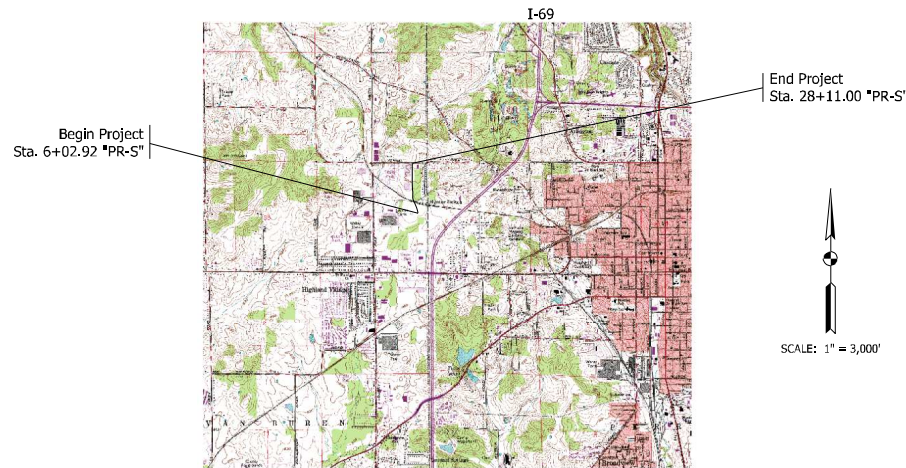
JULIE THOMAS, PRESIDENT DATE
LEE JONES, VICE PRESIDENT DATE
PENNY GITHENS, COMMISSIONER DATE
CATHERINE SMITH, AUDITOR DATE
LISA RIDGE, HIGHWAY DIRECTOR DATE



LATITUDE: 39°10'37"N LONGITUDE: 86°34'35"W

BRIDGE LENGTH: 0.040 MI.
ROADWAY LENGTH: 0.380 MI.
TOTAL LENGTH: 0.420 MI.
MAX. GRADE: 8.94 %

HUC: 05120208090020 & 05120202010080



I-69
MONROE COUNTY

CERTIFIED BY: REGISTERED PROFESSIONAL ENGINEER
STATE OF INDIANA NO. _____ DATE _____
COVERING CIVIL DESIGN

CERTIFIED BY: REGISTERED PROFESSIONAL ENGINEER
STATE OF INDIANA NO. _____ DATE _____
COVERING BRIDGE DESIGN

NOT FOR
CONSTRUCTION
DLZ INDIANA, LLC

NOT FOR
CONSTRUCTION
DLZ INDIANA, LLC

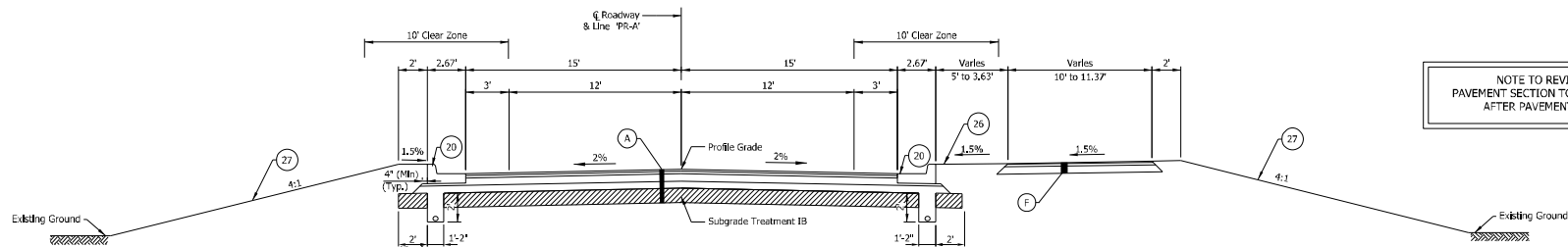


PLANS PREPARED BY:
DLZ INDIANA, LLC
157 E MARYLAND ST.
INDIANAPOLIS, IN 46204
(317)563-4120

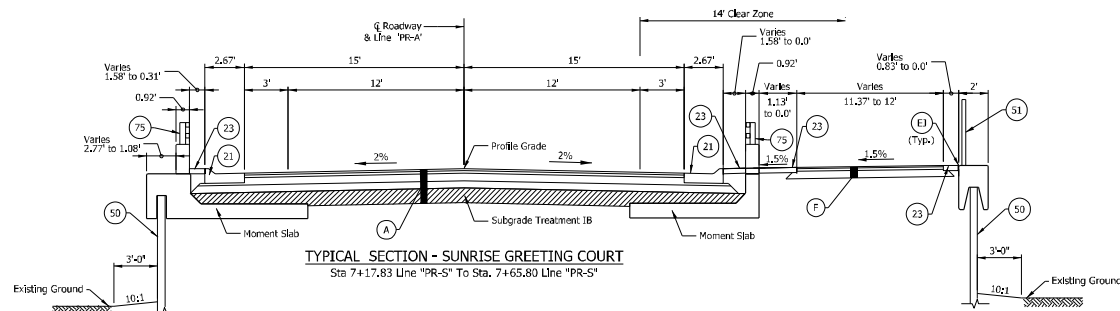
APPROVED FOR LETTING: _____ DATE _____
INDIANA DEPARTMENT OF TRANSPORTATION

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2020
TO BE USED WITH THESE PLANS.

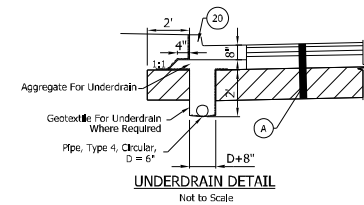
BRIDGE FILE	
DESIGNATION	1702957
SHEETS	
SURVEY BOOK	1 of 56
ELECTRONIC	1 of 56
CONTRACT	PROJECT
R-0090	1702957



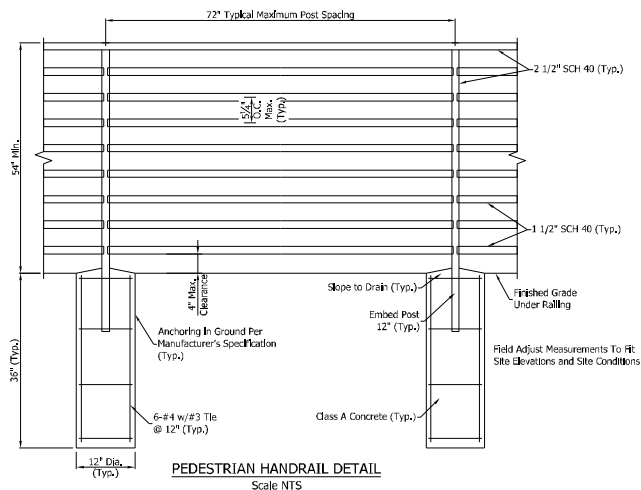
TYPICAL SECTION - SUNRISE GREETING COURT
Sta 6+02.92 Line "PR-S" To Sta. 7+17.83 Line "PR-S"



TYPICAL SECTION - SUNRISE GREETING COURT
Sta 7+17.83 Line "PR-S" To Sta. 7+65.80 Line "PR-S"



UNDERDRAIN DETAIL
Not to Scale



PEDESTRIAN HANDRAIL DETAIL
Scale: NTS

LEGEND:

- (21) Combined Concrete Curb and Gutter, Type B
- (20) Combined Concrete Curb and Gutter, Type C
- (23) 4" PCCP
- (26) Sodding, Nursery
- (27) Seed Mixture, U
- (50) MSE Wall
- (51) Handrail, Pedestrian
- (75) Modified Bridge Railing, Type PF-1
- (77) Impact Attenuator R1=W1, TL=2
- (A) HMA Pavement:
165#/SYD QC/QA-HMA, 3.70, Surface, 9.5 mm, on
275#/SYD QC/QA-HMA, 3.70, Intermediate, 19.0 mm, on
770#/SYD QC/QA-HMA, 3.64, Base, 25.0 mm, on
6" Compacted Aggregate No. 53, on
Subgrade Treatment, Type 1B
- (F) HMA for Sidewalk:
140#/SYD HMA, Surface, Type B, On
220#/SYD HMA, Intermediate, Type B, On
6" Compacted Aggregate No. 53, On
Subgrade Treatment Type III
- (E) Expansion Joint (1/2" Preformed Joint Filler)
See INDOT Standard Drawing E604-CCS2-01.



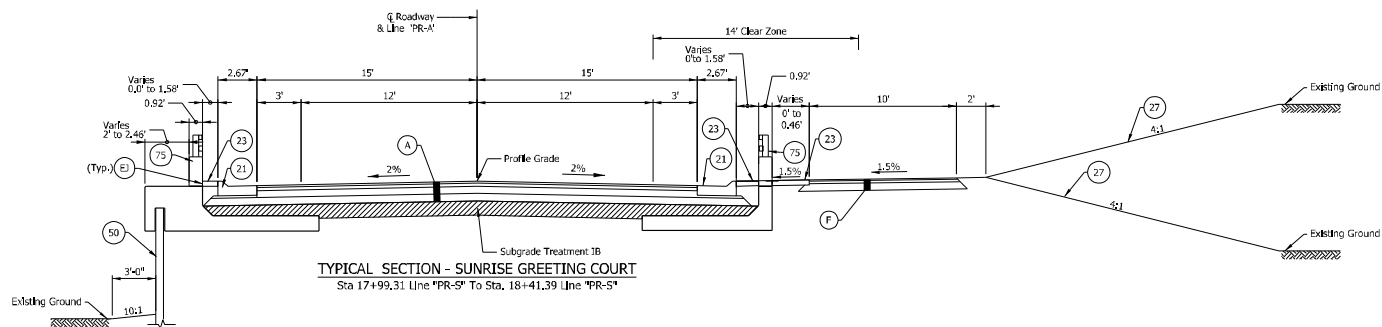
NOT FOR
CONSTRUCTION
DLZ INDIANA, LLC

DESIGNED:	AMH	11/2019	DRAWING:	DPH	11/2019
CHECKED:	DSB	11/2019	CHECKED:	AMH	11/2019

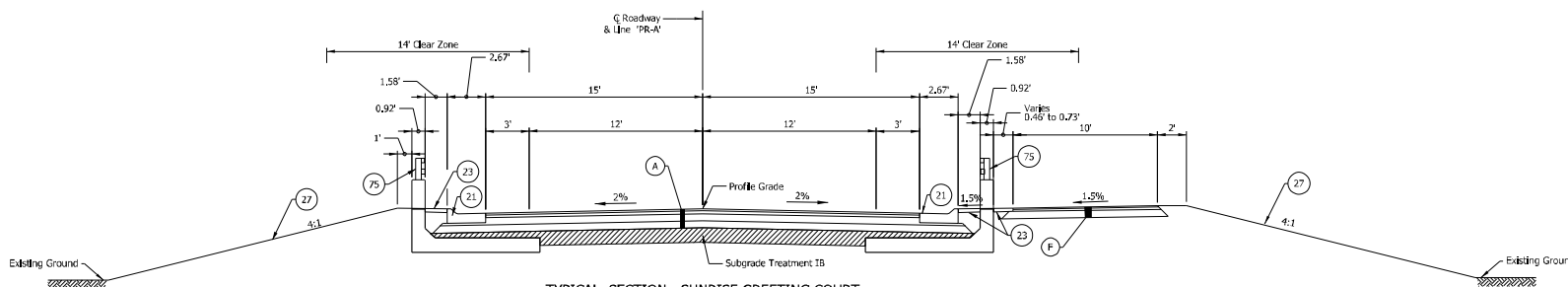
INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

SCALE	BRIDGE FILE
1/4" = 1'-0"	DESIGNATION
	1702957
SURVEY BOOK	SHEETS
ELECTRONIC	4 of 56
CONTRACT	PROJECT
R-1085	1702957



TYPICAL SECTION - SUNRISE GREETING COURT
Sta 17+99.31 Line "PR-S" To Sta. 18+41.39 Line "PR-S"



TYPICAL SECTION - SUNRISE GREETING COURT
Sta 18+41.39 Line "PR-S" To Sta. 18+45.57 Line "PR-S"

LEGEND:

- (20) Combined Concrete Curb and Gutter, Type C
- (21) Combined Concrete Curb and Gutter, Type B
- (23) 4" PCCP
- (26) Sodding, Nursery
- (27) Seed Mixture, U
- (75) Modified Bridge Railing, Type PF-1
- (77) Impact Attenuator R1-W1, TL-2
- (A) HMA Pavement
185#/SYD QC/QA-HMA, 3.70, Surface, 9.5 mm, on
175#/SYD QC/QA-HMA, 3.70, Intermediate, 19.0 mm, on
770#/SYD QC/QA-HMA, 3.64, Base, 25.0 mm, on
Subgrade Treatment, Type IIS
- (F) HMA for Sidewalk
140#/SYD HMA, Surface, Type B, On
220#/SYD HMA, Intermediate, Type B On
6" Compacted Aggregate No 53, On
Subgrade Treatment Type III
- (E) Expansion Joint (1/2" Preformed Joint Filler)
See INDOT Standard Drawing E604-CCSJ-01.



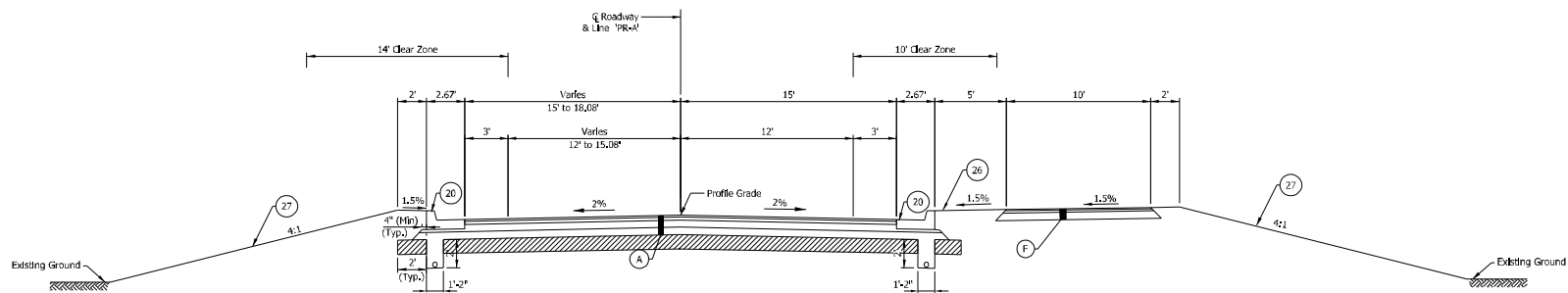
NOT FOR
CONSTRUCTION
DLZ INDIANA, LLC

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: AMH 11/2019	DRAWING: DPH 11/2019	
CHECKED: DSB 11/2019	CHECKED: AMH 11/2019	

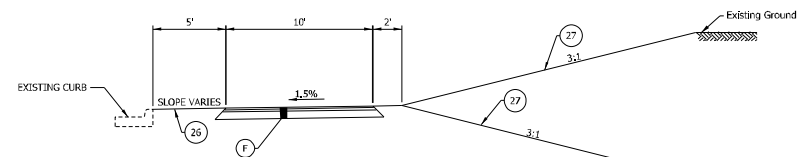
INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

SCALE	BRIDGE FILE
1/4" = 1'-0"	---
DESIGNATION	---
1702957	---
SURVEY BOOK	SHEETS
ELECTRONIC	6 of 56
CONTRACT	PROJECT
R-0880	1702957



TYPICAL SECTION - SUNRISE GREETING COURT
Sta 18+45.57 Line "PR-S" To Sta. 20+83.71 Line "PR-S"



TYPICAL SECTION - SUNRISE GREETING COURT
Sta 21+12.71 Line "PR-S" To Sta. 28+14.66 Line "PR-S"

LEGEND:

- 20 Combined Concrete Curb and Gutter, Type C
- 26 Sodding, Nursery
- 27 Seed Mixture, U
- A HMA Pavement
165#/SYD QC/QA-HMA, 3,70, Surface, 9.5 mm, on
275#/SYD QC/QA-HMA, 3,70, Intermediate, 19.0 mm, on
770#/SYD QC/QA-HMA, 3,64, Base, 25.0 mm, on
6" Compacted Aggregate No. 53, on
Subgrade Treatment, Type IB
- F HMA for Sidewalk
140#/SYD HMA, Surface, Type B, On
220#/SYD HMA, Intermediate, Type B, On
6" Compacted Aggregate No. 53, On
Subgrade Treatment Type III



NOT FOR
CONSTRUCTION
DLZ INDIANA, LLC

DESIGNED: ANH 11/2019	DRAWING: DPH 11/2019
CHECKED: DSB 11/2019	CHECKED: ANH 11/2019

INDIANA
DEPARTMENT OF TRANSPORTATION

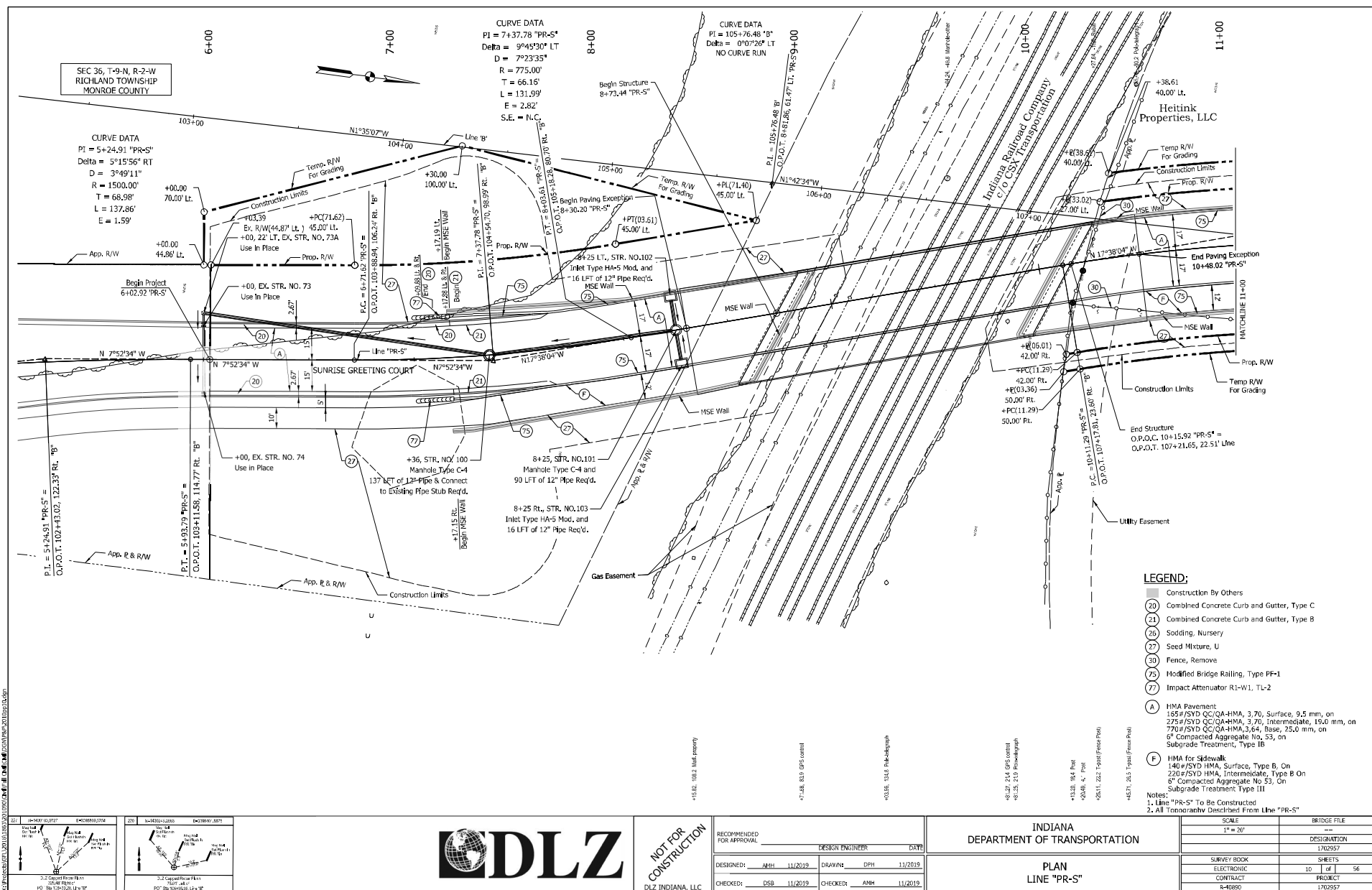
TYPICAL SECTIONS

SCALE 1/4" = 1'-0"	BRIDGE FILE ---
	DESIGNATION 1702957
SURVEY BOOK ELECTRONIC	SHEETS 7 of 56
CONTRACT R-0880	PROJECT 1702957

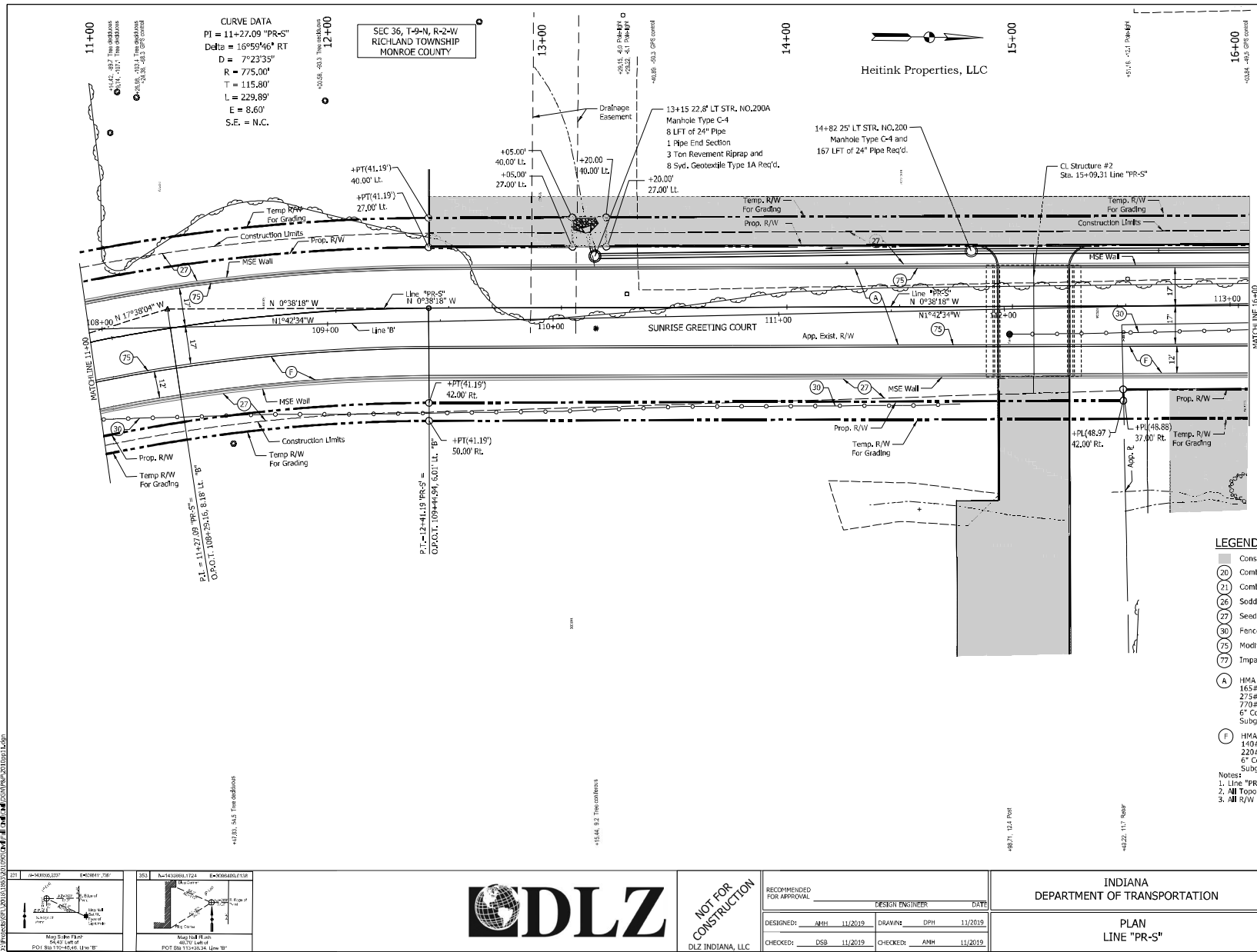
4/6/2020

17/02/2019 11:00:00

17/02/2019 11:00:00



4/6/2020
 INDIANA
 DEPARTMENT OF TRANSPORTATION
 PROJECT: 1702957
 SHEET: 1702957-1
 DRAWING: 1702957-1
 DATE: 4/6/2020



LEGEND:

- Construction By Others
- 20 Combined Concrete Curb and Gutter, Type C
- 21 Combined Concrete Curb and Gutter, Type B
- 26 Sodding, Nursery
- 27 Seed Mixture, U
- 30 Fence, Remove
- 75 Modified Bridge Rating, Type PF-1
- 77 Impact Attenuator R1-W1, TL-2
- A HMA Pavement
 165#/SYD QC/QA-HMA, 3.70, Surface, 9.5 mm, on
 275#/SYD QC/QA-HMA, 3.70, Intermediate, 19.0 mm, on
 770#/SYD QC/QA-HMA, 3.64, Base, 25.0 mm, on
 6\"/>
- F HMA for Sidewalk
 140#/SYD HMA, Surface, Type B, On
 220#/SYD HMA, Intermediate, Type B On
 6\"/>

Notes:
 1. Line "PR-S" To Be Constructed
 2. All Topography Described From Line "PR-S"
 3. All R/W Is Defined From Line "PR-S"



NOT FOR
 CONSTRUCTION
 DLZ INDIANA, LLC

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: AMH 11/2019	DRAWING: DPH 11/2019	
CHECKED: DSB 11/2019	CHECKER: AMH 11/2019	

INDIANA
 DEPARTMENT OF TRANSPORTATION

PLAN
 LINE "PR-S"

SCALE	BRIDGE FILE
1" = 20'	DESIGNATION
	1702957
SURVEY BOOK	SHEETS
ELECTRONIC	12 of 56
CONTRACT	PROJECT
R-0850	1702957

PROJECT	DESIGNATION
1702957	1900406
CONTRACT	BRIDGE FILE
R-0090	---

STRUCTURE INFORMATION				
STRUCTURE	TYPE	SPAN AND SKEW	OVER	STATION
---	Composite Prestressed Concrete Built-T Beam	1 Span: 137'-6" Skew: 37°45'00" Rt.	CSX Railroad	9+44.48 Line "PR-S"

KIN PROJECT INFORMATION	
DESIGNATION	PROJECT DESCRIPTION
1702957	VERNAL PIKE CONNECTOR ROAD (LEAD)
1900406	VERNAL PIKE CONNECTOR BRIDGE OVER RAILROAD
1092093	VERNAL PIKE CONNECTOR PARKING LOT CONNECTOR TUNNEL

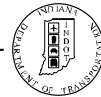
MONROE COUNTY BOARD OF COMMISSIONERS

JULIE THOMAS, PRESIDENT	DATE
LEE JONES, VICE PRESIDENT	DATE
PENNY GITHENS, COMMISSIONER	DATE
CATHERINE SMITH, AUDITOR	DATE
LISA RIDGE, HIGHWAY DIRECTOR	DATE

CERTIFIED BY:	REGISTERED PROFESSIONAL ENGINEER STATE OF INDIANA NO. 1010213 COVERING CIVIL DESIGN	DATE
CERTIFIED BY:	REGISTERED PROFESSIONAL ENGINEER STATE OF INDIANA NO. 910382 COVERING BRIDGE DESIGN	DATE

NOT FOR CONSTRUCTION	NOT FOR CONSTRUCTION
DLZ INDIANA, LLC	DLZ INDIANA, LLC

INDIANA DEPARTMENT OF TRANSPORTATION



BRIDGE PLANS

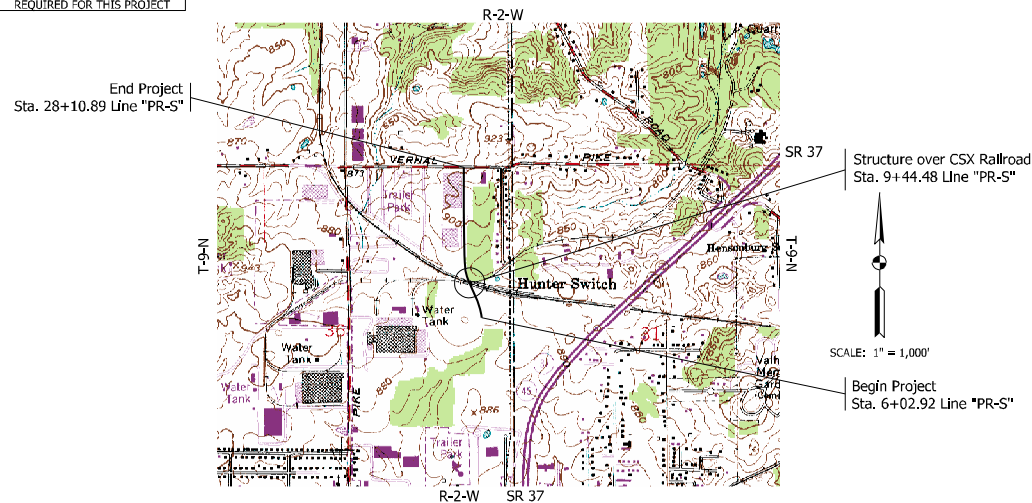
FOR SPANS OVER 20 FEET
ROUTE: VERNAL PIKE CONNECTOR

STAGE 2 PLANS
APRIL 7, 2020

PROJECT NO. 1900406 P.E.
1702957 R/W
1702957 CONST.

New Bridge Construction on Vernal Pike Connector over CSX Railroad
Located Approx. 0.5 miles west of SR 37 In
Section 36, T-9-N, R-2-W, Richland Township, Monroe County, Indiana

ADDITIONAL RIGHT-OF-WAY
REQUIRED FOR THIS PROJECT

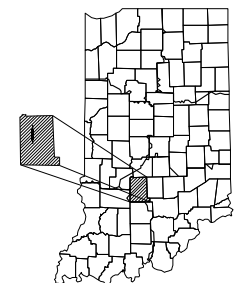


TRAFFIC DATA - VERNAL PIKE CONNECTOR

A.A.D.T. (2041)	5,530 V.P.D.
D.H.V (2041)	V.P.H.
DIRECTIONAL DISTRIBUTION	50%
TRUCKS	10% A.A.D.T.
	% D.H.V.

DESIGN DATA - VERNAL PIKE CONNECTOR

DESIGN SPEED	35 M.P.H.
PROJECT DESIGN CRITERIA	NEW CONSTRUCTION (NON-FREIGHT)
FUNCTIONAL CLASSIFICATION	LOCAL AGENCY COLLECTOR
RURAL/URBAN	URBAN
TERRAIN	LEVEL
ACCESS CONTROL	NONE



PROJECT LOCATION SHOWN BY
Monroe County

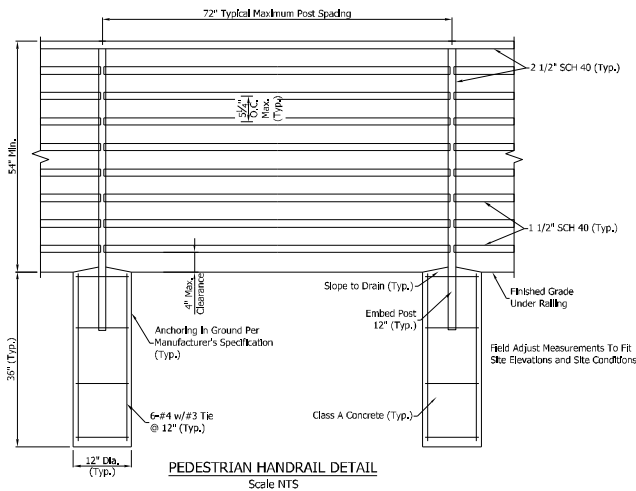
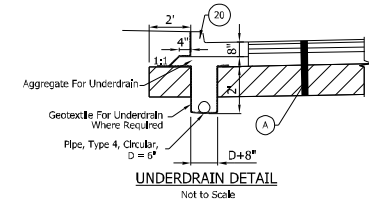
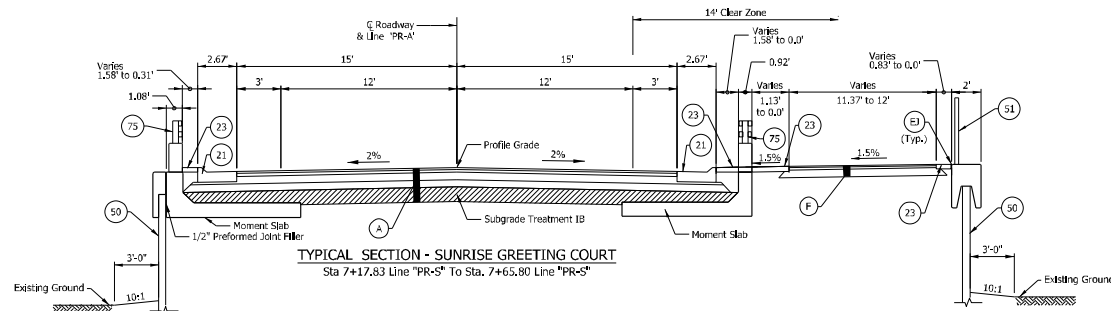
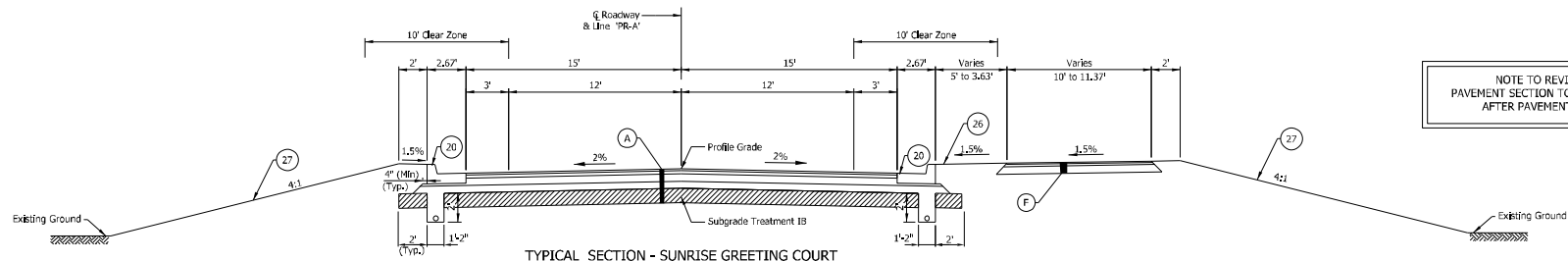
LATITUDE: 39°10'26" LONGITUDE: 86°34'34"

BRIDGE LENGTH:	0.022 MI.
TUNNEL LENGTH:	0.007 MI.
ROADWAY LENGTH:	0.384 MI.
TOTAL LENGTH:	0.418 MI.
MAX. GRADE:	8.94 %

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2020
TO BE USED WITH THESE PLANS.

BRIDGE FILE	
DESIGNATION	
1900406	
SURVEY BOOK	SHEETS
ELECTRONIC	1 of 16
CONTRACT	PROJECT
R-0090	1702957

APPROVED FOR LETTING: _____ DATE _____
INDIANA DEPARTMENT OF TRANSPORTATION



- LEGEND:**
- (21) Combined Concrete Curb and Gutter, Type B
 - (20) Combined Concrete Curb and Gutter, Type C
 - (23) 4" PCCP
 - (26) Sodding, Nursery
 - (27) Seed Mixture, U
 - (50) MSE Wall
 - (51) Handrail, Pedestrian
 - (75) Modified Bridge Railing, Type PF+1
 - (77) Impact Attenuator R1-W1, TL-2
 - (A) HMA Pavement:
165# SYD QC/QA-HMA, 3.70, Surface, 9.5 mm, on
275# SYD QC/QA-HMA, 3.70, Intermediate, 19.0 mm, on
770# SYD QC/QA-HMA, 3.64, Base, 25.0 mm, on
6" Compacted Aggregate No. 53, on
Subgrade Treatment, Type IB
 - (F) HMA for Sidewalk:
140# SYD HMA, Surface, Type B, On
220# SYD HMA, Intermediate, Type B On
6" Compacted Aggregate No. 53, On
Subgrade Treatment Type III
 - (E) Expansion Joint (1/2" Preformed Joint Filler)
See INDOT Standard Drawing E604-CCS-01.

FOR INFORMATION ONLY



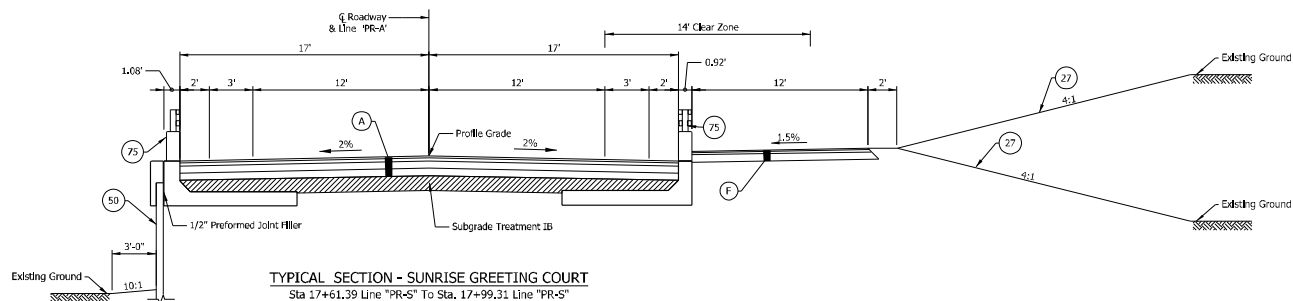
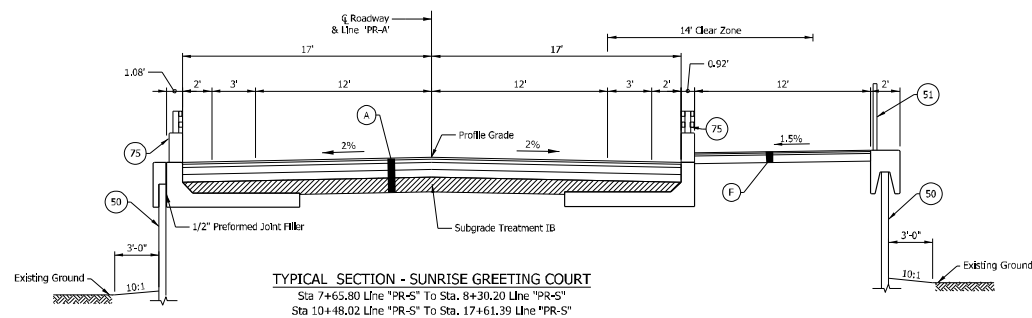
NOT FOR
CONSTRUCTION
DLZ INDIANA, LLC

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: AMH 4/2020	DRAWING: DPH 4/2020	
CHECKED: DSB 4/2020	CHECKED: AMH 4/2020	

INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

SCALE	BRIDGE FILE
1/4" = 1'-0"	
DESIGNATION	
1900406	
SURVEY BOOK	SHEETS
ELECTRONIC	3 of 16
CONTRACT	PROJECT
R-1085	1702957



LEGEND:

- | | |
|----|--|
| 21 | Combined Concrete Curb and Gutter, Type B |
| 23 | 4" PCPP |
| 26 | Sodding, Nursery |
| 27 | Seed Mixture, U |
| 50 | MSE Wall |
| 51 | Handrail, Pedestrian |
| 75 | Modified Bridge Railing, Type PF-1 |
| 77 | Impact Attenuator R1-W1, TL-2 |
| A | HMA Pavement:
165 for SYD Q/CQA-HMA, 3.70, Surface, 9.5 mm, on
8" Compact Q/CQA-HMA, 3.70, Intermediate, 19.0 mm, on
770 for SYD Q/CQA-HMA, 3.64, Base, 25.0 mm, on
8" Compact Aggregate No. 53, on
Subgrade Treatment, Type IB |
| E | HMA for Sidewalk:
140 for SYD HMA, Surface, Type B, On
220 for SYD HMA, Intermediate, Type B On
8" Compact Aggregate No. 53, On
Subgrade Treatment, Type III |

FOR INFORMATION ONLY



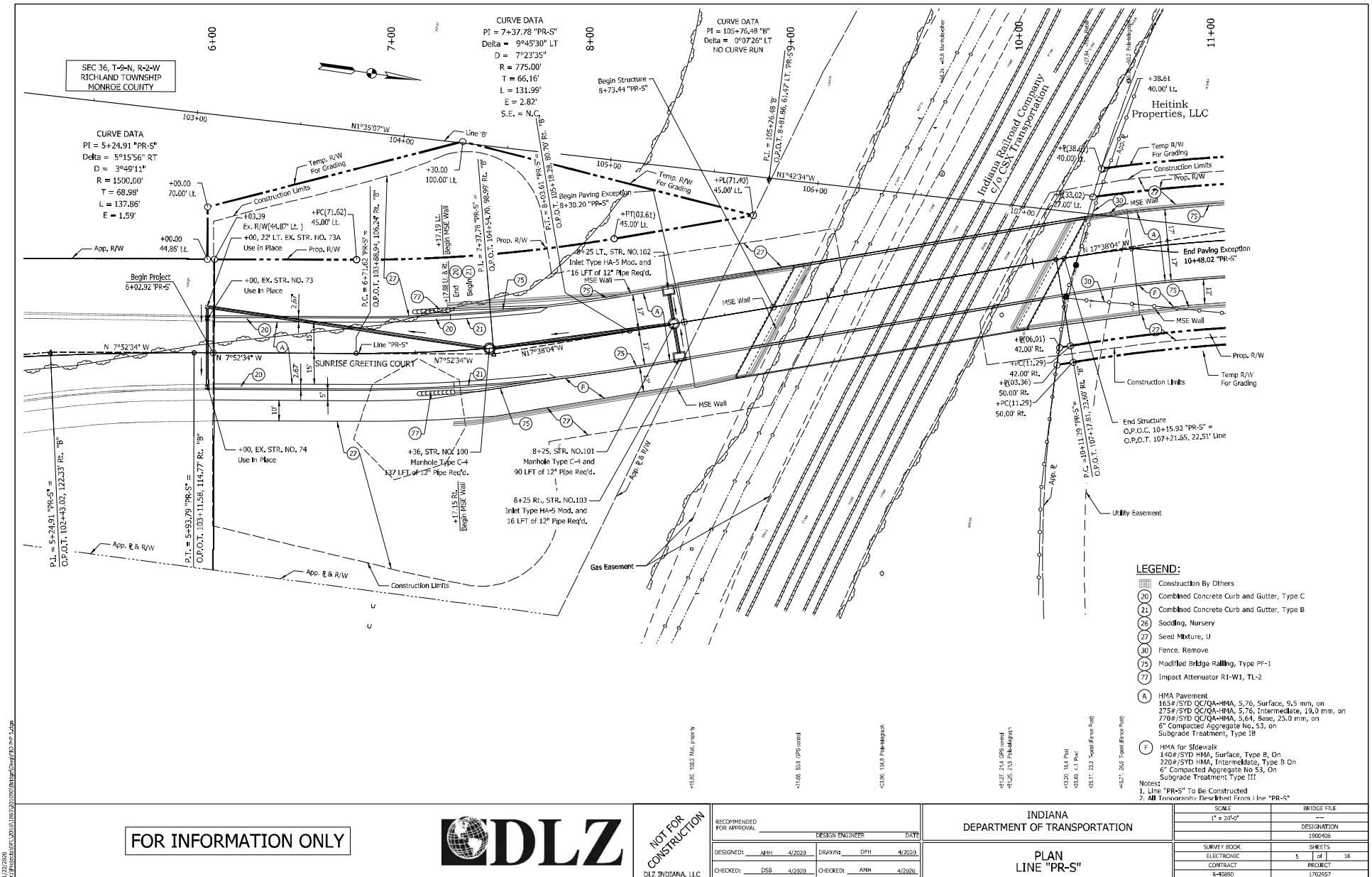
NOT FOR
CONSTRUCTION

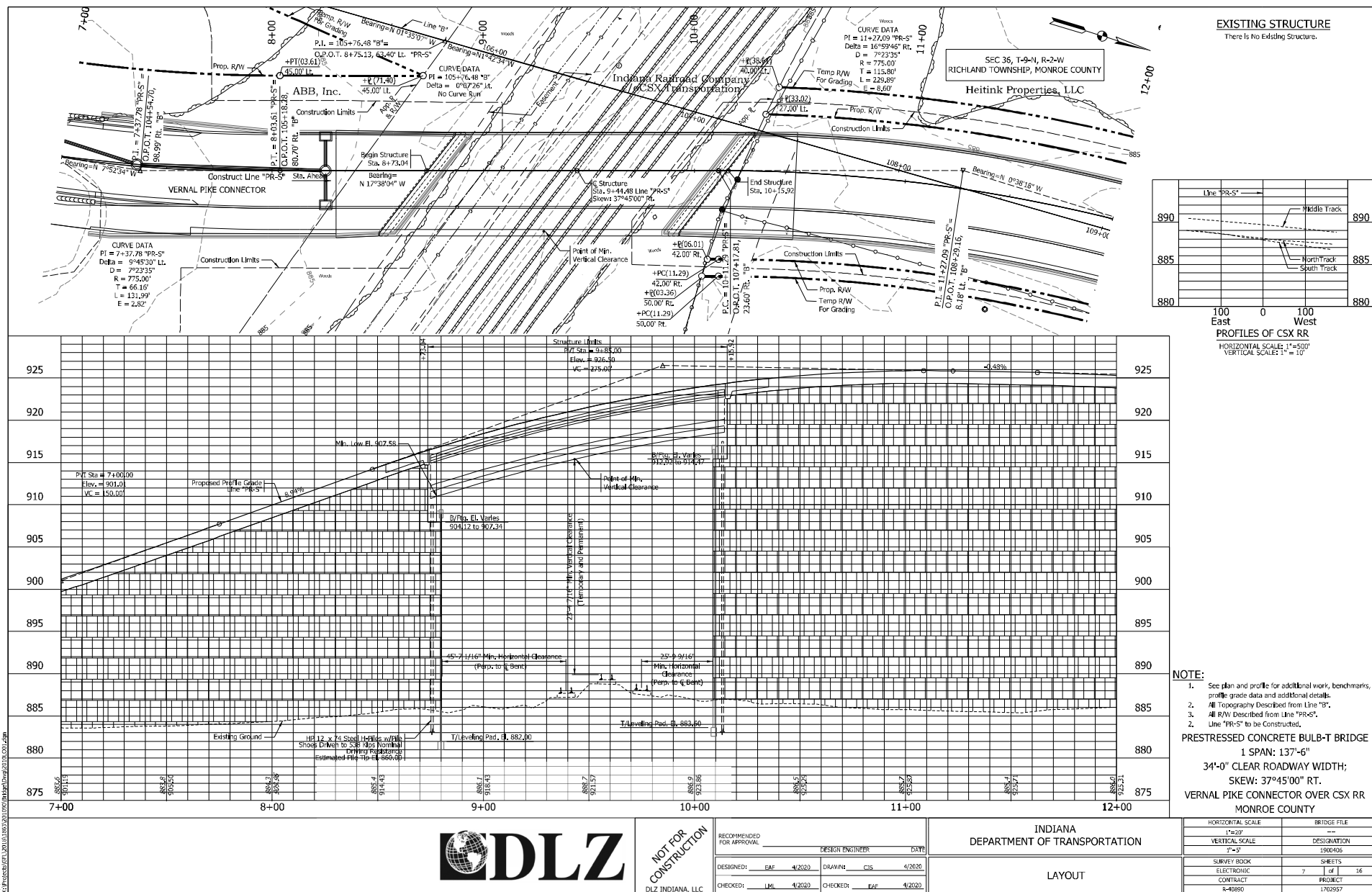
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: AMH 4/2020	DRAWING: DPH	4/2020
CHECKED: DSB 4/2020	CHECKED: AMH	4/2020

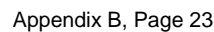
INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

SCALE		BRIDGE FILE	
1/4" = 1'-0"		---	
		DESIGNATION	
		1900406	
SURVEY BOOK		SHEETS	
ELECTRONIC		4	of 16
CONTRACT		PROJECT	
R-0890		1702957	







PROJECT	DESIGNATION
1702957	1902093
CONTRACT	BRIDGE FILE
R-0090	---

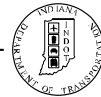
STRUCTURE INFORMATION				
STRUCTURE	TYPE	SPAN AND SKEW	OVER	STATION
---	Precast Reinforced Concrete Three-Sided Structure	1 Span: 32'-0" Skew: 0°0'00"	Parking Lot Connector	15+09.31 Line "PR-S"

KIN PROJECT INFORMATION	
DESIGNATION	PROJECT DESCRIPTION
1702957	VERNAL PIKE CONNECTOR ROAD (LEAD)
1900406	VERNAL PIKE CONNECTOR BRIDGE OVER RAILROAD
1092093	VERNAL PIKE CONNECTOR PARKING LOT CONNECTOR TUNNEL

MONROE COUNTY BOARD OF COMMISSIONERS

JULIE THOMAS, PRESIDENT	DATE
LEE JONES, VICE PRESIDENT	DATE
PENNY GITHENS, COMMISSIONER	DATE
CATHERINE SMITH, AUDITOR	DATE
LISA RIDGE, HIGHWAY DIRECTOR	DATE

INDIANA DEPARTMENT OF TRANSPORTATION



BRIDGE PLANS

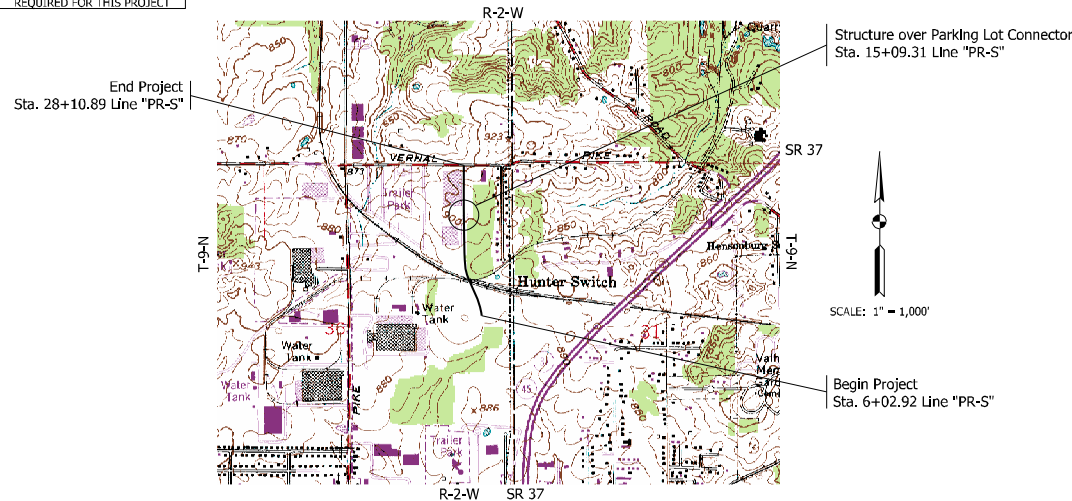
FOR SPANS OVER 20 FEET
ROUTE: VERNAL PIKE CONNECTOR

STAGE 2 PLANS
APRIL 7, 2020

PROJECT NO. 1902093 P.E.
1702957 R/W
1702957 CONST.

New Tunnel on Parking Lot Connector Under Vernal Pike Connector
Located Approx. 0.5 miles west of SR 37 In
Section 36, T-9-N, R-2-W, Richland Township, Monroe County, Indiana

ADDITIONAL RIGHT-OF-WAY
REQUIRED FOR THIS PROJECT

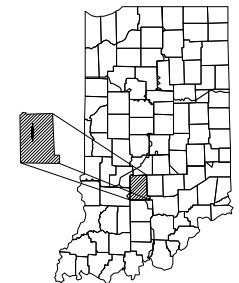


TRAFFIC DATA - VERNAL PIKE CONNECTOR

A.A.D.T. (2041)	5,530 V.P.D.
D.H.V. (2041)	V.P.H.
DIRECTIONAL DISTRIBUTION	50%
TRUCKS	10% A.A.D.T.
	% D.H.V.

DESIGN DATA - VERNAL PIKE CONNECTOR

DESIGN SPEED	35 M.P.H.
PROJECT DESIGN CRITERIA	NEW CONSTRUCTION (NON-FREIGHT)
FUNCTIONAL CLASSIFICATION	LOCAL AGENCY COLLECTOR
RURAL/URBAN	URBAN
TERRAIN	LEVEL
CROSS CONTROL	NONE



PROJECT LOCATION SHOWN BY
Monroe County

LATITUDE: 39°10'41" LONGITUDE: 86°34'35"

BRIDGE LENGTH:	0.022 MI.
TUNNEL LENGTH:	0.007 MI.
ROADWAY LENGTH:	0.384 MI.
TOTAL LENGTH:	0.418 MI.
MAX. GRADE:	8.94 %

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2020
TO BE USED WITH THESE PLANS.

CERTIFIED BY:	REGISTERED PROFESSIONAL ENGINEER STATE OF INDIANA NO. 1010213 COVERING CIVIL DESIGN	DATE
CERTIFIED BY:	REGISTERED PROFESSIONAL ENGINEER STATE OF INDIANA NO. 910382 COVERING BRIDGE DESIGN	DATE

NOT FOR
CONSTRUCTION

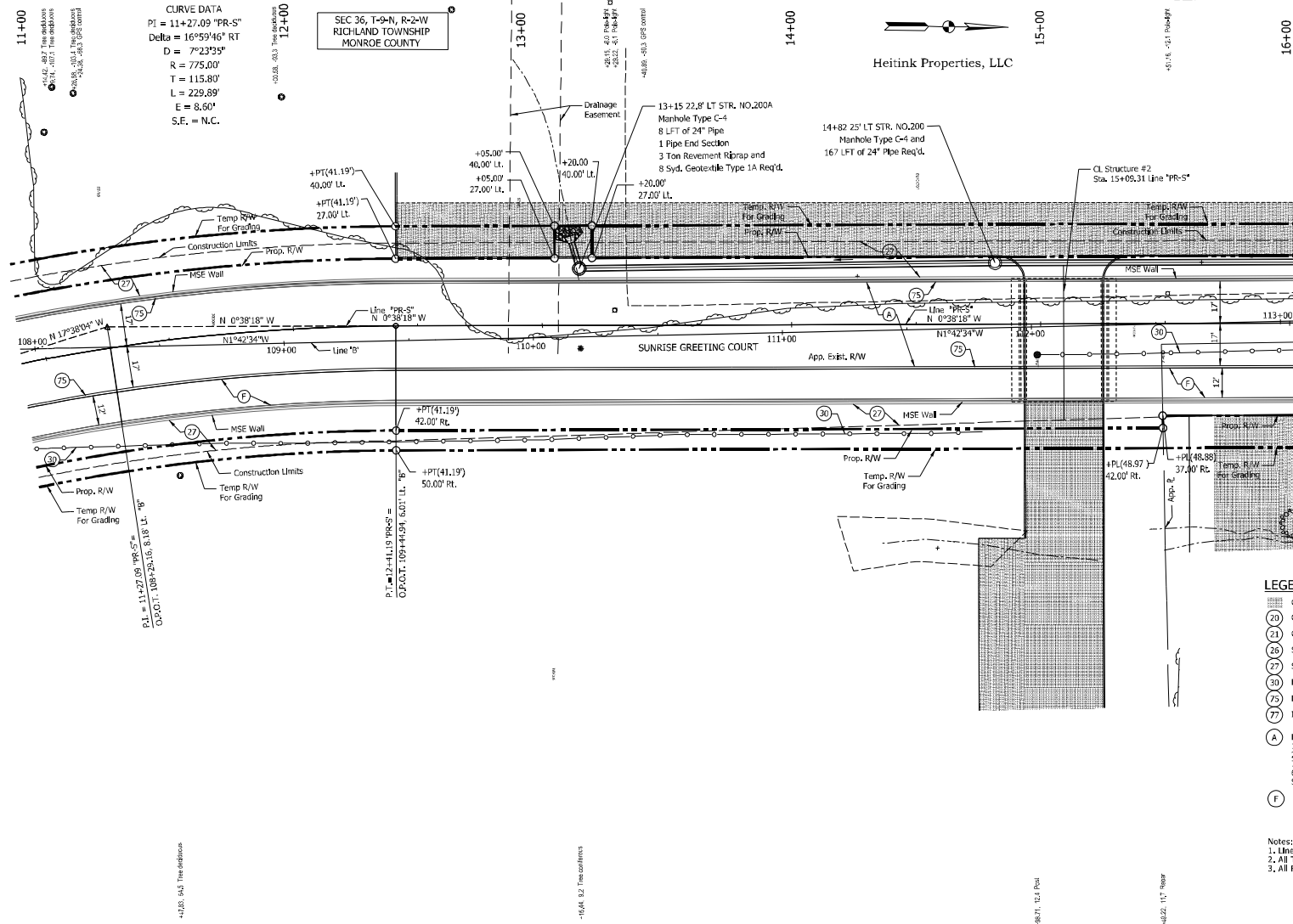
DLZ INDIANA, LLC



PLANS PREPARED BY:
DLZ INDIANA, LLC
157 EAST HAWKINS STREET
INDIANAPOLIS, INDIANA 46204-3608
(317) 633-4120

APPROVED FOR LETTING: _____ DATE _____
INDIANA DEPARTMENT OF TRANSPORTATION

BRIDGE FILE	
DESIGNATION	
1902093	
SHEETS	
SURVEY BOOK	1 of 11
ELECTRONIC	1 of 11
CONTRACT	PROJECT
R-0090	1702957



- LEGEND:**
- Construction By Others
 - (20) Combined Concrete Curb and Gutter, Type C
 - (21) Combined Concrete Curb and Gutter, Type B
 - (26) Sodding, Nursery
 - (27) Seed Mixture, U
 - (30) Fence, Remove
 - (75) Modified Bridge Railing, Type PF-1
 - (77) Impact Attenuator R1-W1, TL-2
 - (A) HMA Pavement
165#/SYD QC/QA-HMA, 5.76, Surface, 9.5 mm, on 275#/SYD QC/QA-HMA, 5.76, Intermediate, 19.0 mm, on 770#/SYD QC/QA-HMA, 5.64, Base, 25.0 mm, on 6" Compacted Aggregate No. 53, on Subgrade Treatment, Type IB
 - (F) HMA for Sidewalk
140#/SYD HMA, Surface, Type B, On 220#/SYD HMA, Intermediate, Type B On 6" Compacted Aggregate No. 53, On Subgrade Treatment Type III
- Notes:**
1. Line "PR-S" To Be Constructed
 2. All Topography Described From Line "PR-S"
 3. All R/W is Defined from Line "PR-S"

FOR INFORMATION ONLY



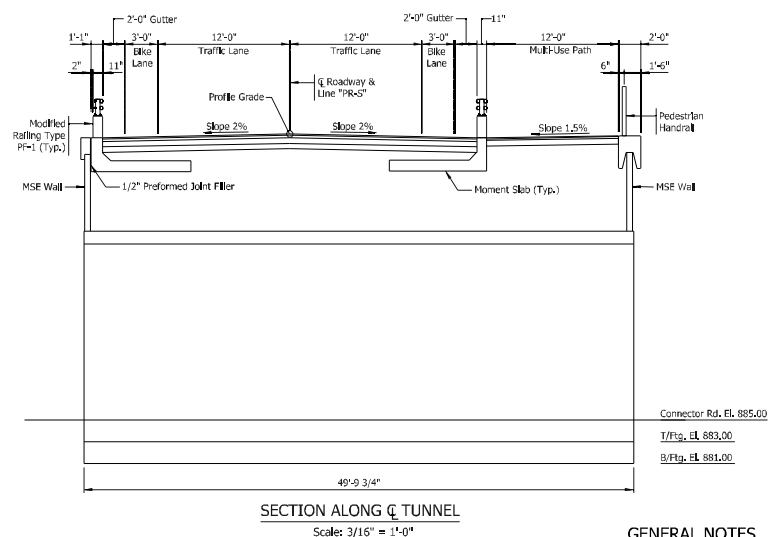
NOT FOR CONSTRUCTION
DLZ INDIANA, LLC

DESIGNED: AMH 4/2020	DRAWING: DPH 4/2020
CHECKED: DSB 4/2020	CHECKED: AMH 4/2020

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN
LINE "PR-S"

SCALE 1" = 20'-0"	BRIDGE FILE
DESIGNATION 1900406	
SURVEY BOOK	SHEETS
ELECTRONIC 4	of 11
CONTRACT R-0080	PROJECT 1702957



1. Reinforcing steel covering shall be 3" in footings, except bottom steel which shall be 4", and 2" in all other parts unless noted.
2. Factored soil bearing resistance for footings is psf .
3. An alternate three-sided arch-topped structure type with a 32 ft perpendicular span and a 21 ft. rise may be substituted for the structure shown on the layout sheet.

Designed for HL-93 loading, in accordance with AASHTO LRFD Bridge Design Specifications, Eighth Edition, 2017.

Actual Dead load will consist of the culvert, 9.4' max. of earth cover plus 35 psf for future wearing surface.

CAST-IN-PLACE CONCRETE:

Class C	$f_c = 4,000 \text{ psi}$
Class B	$f_c = 3,000 \text{ psi}$
Class A	$f_c = 3,500 \text{ psi}$

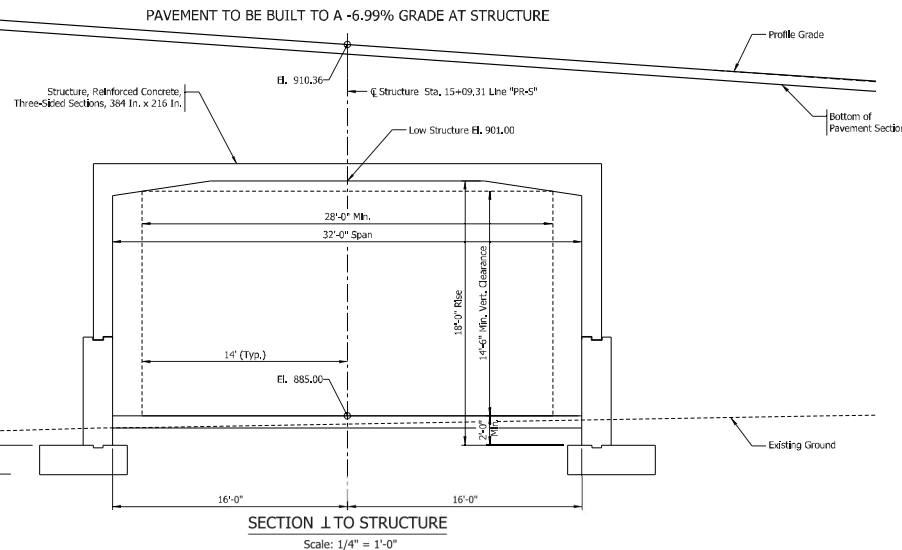
REINFORCING STEEL:

(Grade 60) $f_y = 60,000 \text{ psi}$

PRECAST CULVERT

Precast Concrete $f'_c = 5,000$ psi
Reinforcing Steel (Grade 60) $f_y = 60,000$ psi

PRECAST REINFORCED CONCRETE
THREE-SIDED STRUCTURE
1 SPAN: 32'-0"
34'-0" CLEAR ROADWAY WIDTH; 0° SKEW
VERNAL PIKE CONNECTOR OVER
PARKING LOT CONNECTOR
MONROE COUNTY



NOT FOR
CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: LML	4/2020	DRAWING: CJS 4/2020
CHECKED: LML	4/2020	CHECKED: LML 4/2020

INDIANA
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN

SCALE		BRIDGE FILE	
AS NOTED		---	
		DESIGNATION	
		1906406	
SURVEY BOOK		SHEETS	
ELECTRONIC		7	of 11
CONTRACT		PROJECT	
S-Anderson		1707057	

APPROVED BY MONROE COUNTY
BOARD OF COMMISSIONERS

Julie Thomas 7/31
JULIE THOMAS, PRESIDENT DATE

Lee Jones 7/31
LEE JONES, COMMISSIONER DATE

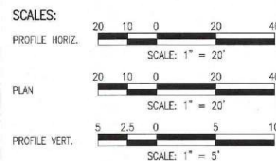
Penny Othens 7/31
PENNY OTHENS, COMMISSIONER DATE

Katherine Smith 7/31/19
KATHERINE SMITH, CLERK DATE

Lisa Ridge 7/31
LISA RIDGE, HIGHWAY DIRECTOR DATE

MONROE COUNTY PROFILE PARKWAY EXTENSION ROAD PLANS

EXTENSION OF PROFILE PARKWAY FROM CURRY PIKE TO GATES DRIVE.
APPROX. 0.5 MILES WEST OF I-69 IN SECTION 36, T9N, R2W, RICHLAND
TOWNSHIP, MONROE COUNTY, INDIANA



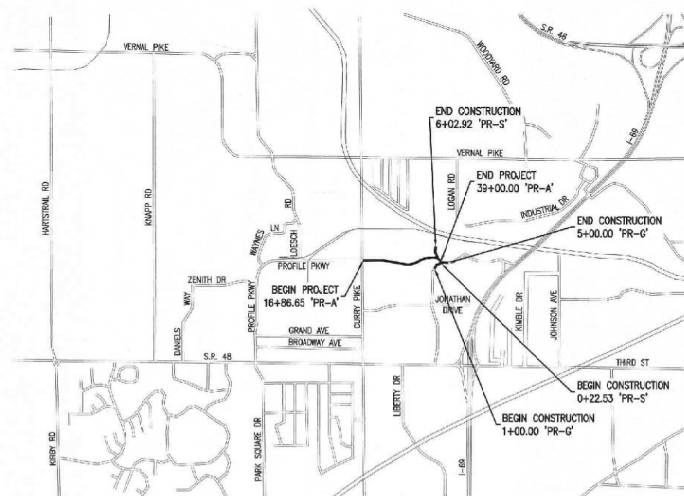
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

David K. Fink 08/21/2019
REGISTERED PROFESSIONAL ENGINEER
STATE OF INDIANA NO. 860383
COVERING OVERALL DESIGN

Don Barth 8/21/19
REGISTERED PROFESSIONAL ENGINEER
STATE OF INDIANA NO. 10708201
COVERING CIVIL DESIGN

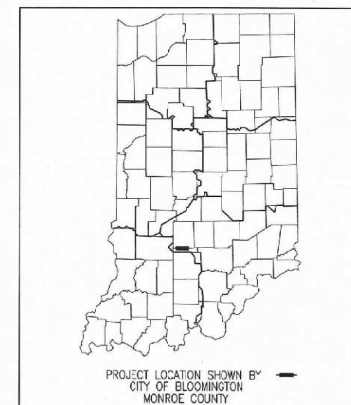
Michael P. Goff 8/21/19
REGISTERED PROFESSIONAL ENGINEER
STATE OF INDIANA NO. 11400296
COVERING SANITARY & WATERLINE DESIGN

Jeffrey R. Anderson 8/21/19
REGISTERED PROFESSIONAL ENGINEER
STATE OF INDIANA NO. 80900360
COVERING TRAFFIC & LIGHTING DESIGN



MONROE COUNTY
SCALE: 1" = 1500'

TRAFFIC DATA	PROFILE PARKWAY
A.A.D.T. (2035 PROJECTED)	8,000 (P.D.)
DESIGN DATA	PROFILE PARKWAY
DESIGN SPEED	35 mph
PROJECT DESIGN CRITERIA	NEW CONSTRUCTION (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION	MAJOR COLLECTOR
RURAL/URBAN	URBAN
TERRAIN	LEVEL
ACCESS CONTROL	NONE



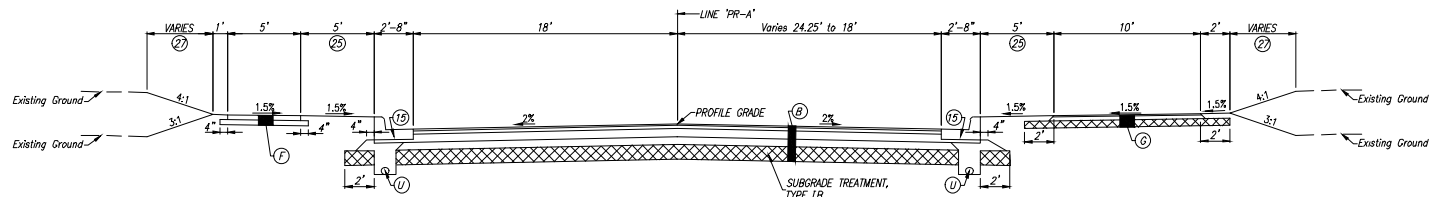
LATITUDE 39°10'19" N LONGITUDE 86°34'56" W

GROSS LENGTH: 0.59 MI.
NET LENGTH: 0.59 MI.
MAX. GRADE: 8%

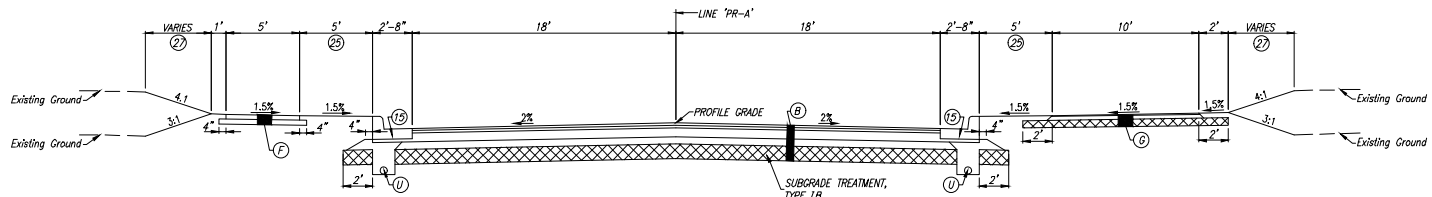
INDIANA DEPARTMENT OF TRANSPORTATION STANDARD
SPECIFICATIONS DATED 2018 AND CURRENT SUPPLEMENTS
THEREOF, MANUAL FOR CONSTRUCTION WITHIN AND ADJACENT
TO MONROE COUNTY RIGHT-OF-WAY, AND CONSTRUCTION
SPECIFICATIONS FOR CITY OF BLOOMINGTON UTILITIES TO BE
USED WITH THESE PLANS



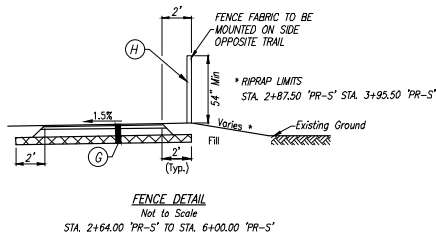
BIDGE FILE	
DESIGNATION	
SURVEY BOOK	SHEETS
ELECTRONIC	1 OF 1
CONTRACT	88
	PROJECT



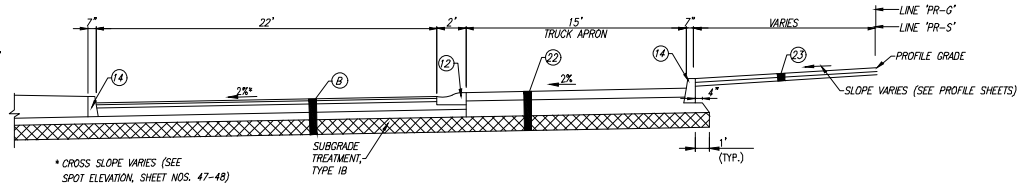
TYPICAL SECTION - PROFILE PARKWAY
STA 17+49.58 'PR-A' TO STA 18+01.19 'PR-A'



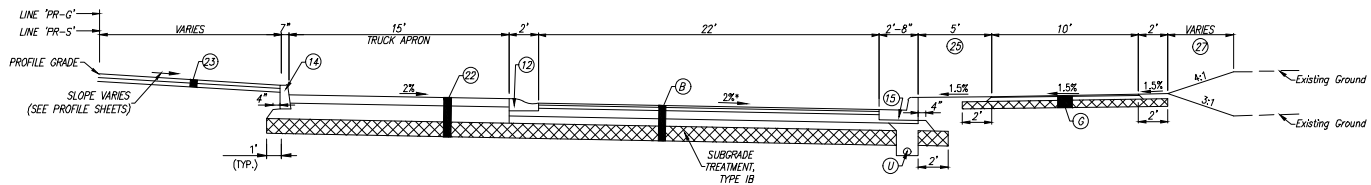
TYPICAL SECTION - PROFILE PARKWAY
STA 18+01.19 'PR-A' TO STA 34+78.91 'PR-A'



STA 2+64.00 'PR-S' TO STA 6+00.00 'PR-S'



TYPICAL SECTION - ROUNDABOUT
STA 1+86.85 'PR-G' TO STA 3+06.90 'PR-G' (LT.)
STA 2+82.77 'PR-S' TO STA 4+02.77 'PR-S' (LT.)



TYPICAL SECTION - ROUNDABOUT
STA 1+86.85 'PR-G' TO STA 3+06.90 'PR-G' (RT.)
STA 2+82.77 'PR-S' TO STA 4+02.77 'PR-S' (RT.)

LEGEND

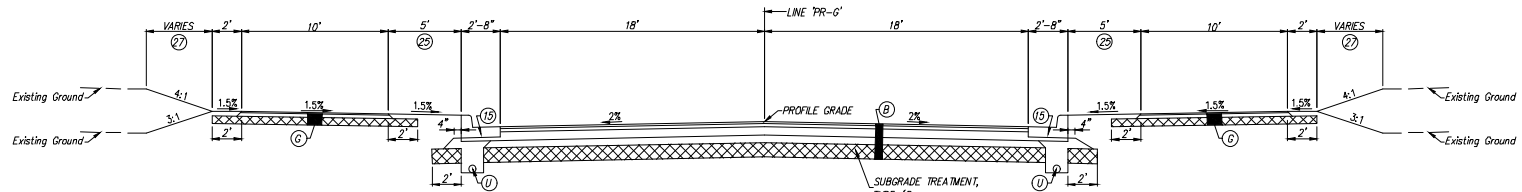
- (2) ROLLED CURB (SLOPING)
- (4) CONCRETE CURB (VERTICAL)
- (15) COMBINED CONCRETE CURB AND GUTTER, TYPE C
- (22) 7" PCPP, COLORED AND STAMPED
(WITH 14" D-1 JOINT SPACING AND 1" DOWEL BARS) ON
1.5" COMPACTED AGGREGATE, NO. 53, ON
SUBGRADE TREATMENT, TYPE 1B
- (23) 4" PCPP, COLORED AND STAMPED
ON 4" COMPACTED AGGREGATE, NO. 53
- (26) SODDING, NURSERY
- (27) SEED MIXTURE, U
- (B) HMA PAVEMENT
165#/SYD HMA, SURFACE, TYPE C, 9.5 mm, ON
275#/SYD HMA, INTERMEDIATE, TYPE C, 19.0 mm, ON
770#/SYD HMA, BASE, TYPE C, 25.0 mm, ON
6" COMPACTED AGGREGATE, NO. 53, ON
SUBGRADE TREATMENT, TYPE 1B
- (F) CONCRETE SIDEWALK, 4" ON
4" COMPACTED AGGREGATE, NO. 53
- (G) HMA FOR SIDEWALK
110#/SYD HMA, SURFACE, TYPE B, ON
330#/SYD HMA, INTERMEDIATE, TYPE B, ON
6" COMPACTED AGGREGATE, NO. 53
- (H) CHAINLINK FENCE, MODIFIED FOR
DETAILS SEE THIS SHEET AND 57.
- (U) UNDERDRAIN, SEE DETAIL, SHEET 65



RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: DJM	DRAWN: DJM	
CHECKED: DSB	CHECKED: FS	

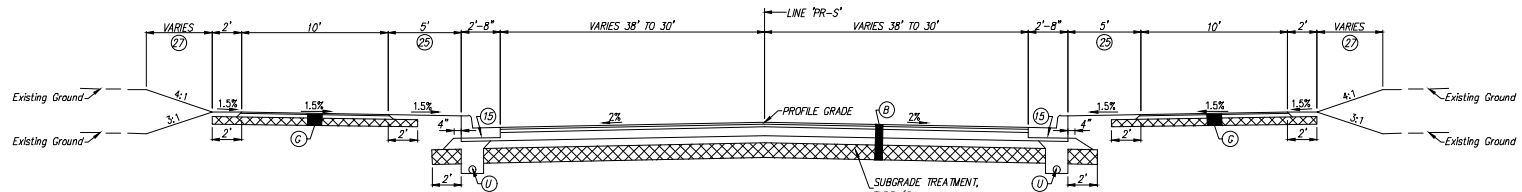
MONROE COUNTY PROFILE PARKWAY EXTENSION
TYPICAL SECTIONS PROFILE PARKWAY

HORIZONTAL SCALE	ROAD FILE
N.T.S.	DESIGNATION
VERTICAL SCALE	
N.T.S.	
SURVEY BOOK	SHEETS
ELECTRONIC	3 of 88
CONTRACT	PROJECT
	1763-1207-90

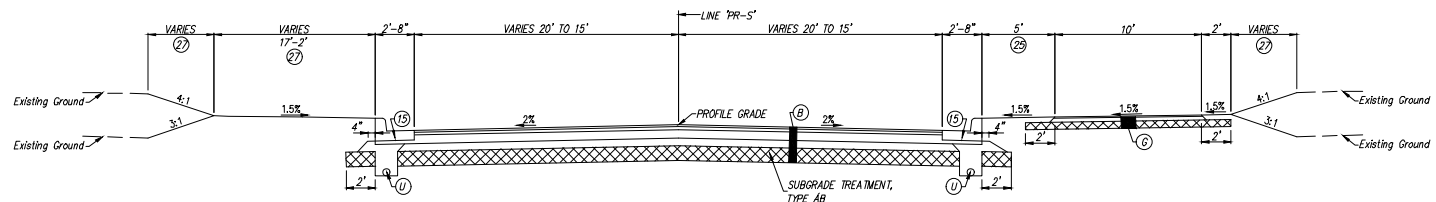


*Varies 10' to 5' from STA. 4+04.53 'PR-G' TO STA. 4+53.24 'PR-G'
 *5' from STA. 4+53.24 'PR-G' TO STA. 5+00.00 'PR-G'

TYPICAL SECTION - ROUNDABOUT APPROACH
 STA. 1+00.00 'PR-G' TO STA. 1+24.88 'PR-G'
 STA. 3+89.30 'PR-G' TO STA. 5+00.00 'PR-G'



TYPICAL SECTION - ROUNDABOUT APPROACH
 STA. 4+71.02 'PR-S' TO STA. 4+75.32 'PR-S'



TYPICAL SECTION - ROUNDABOUT APPROACH
 STA. 4+75.32 'PR-S' TO STA. 6+02.92 'PR-S'

LEGEND

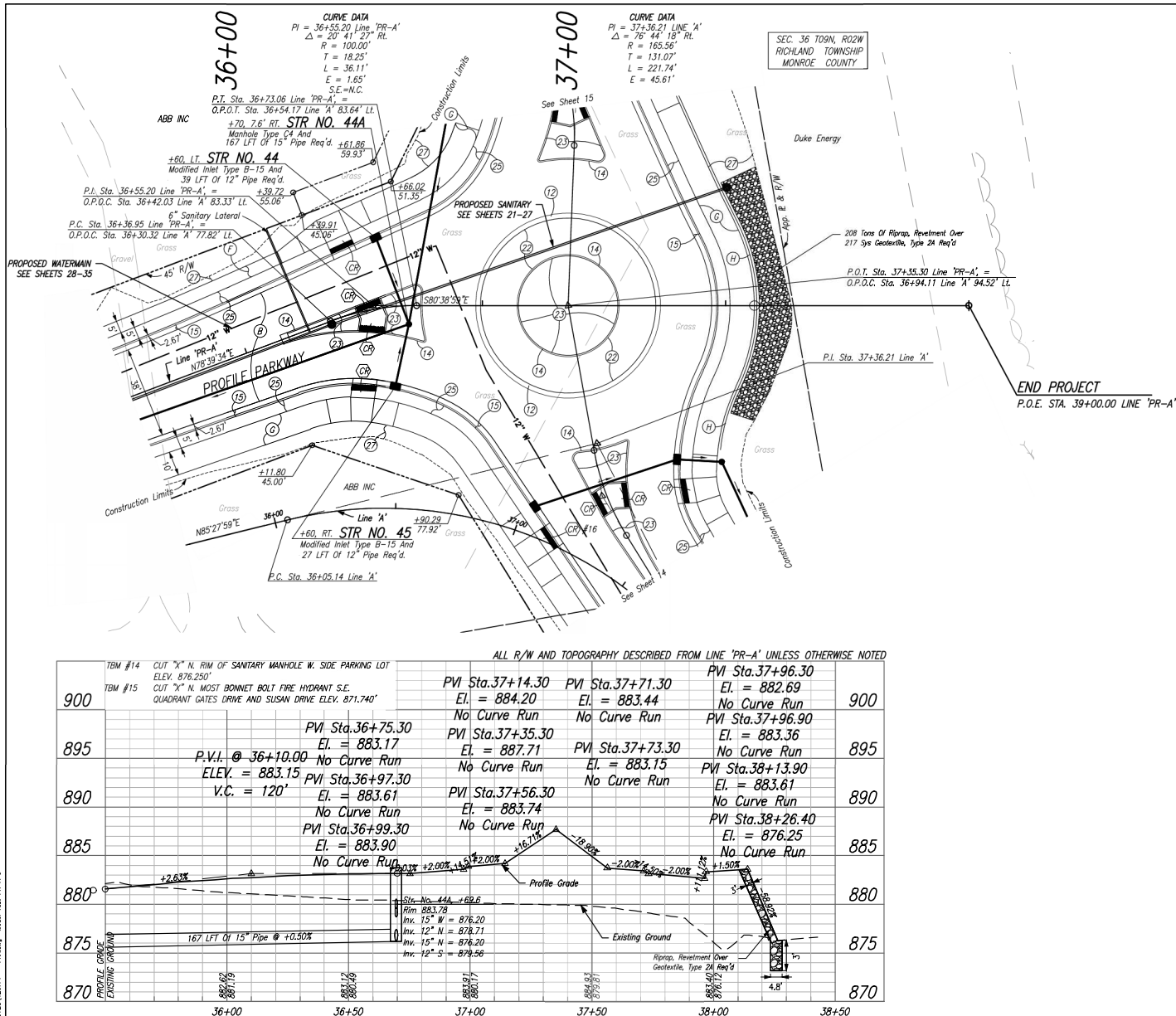
- (15) COMBINED CONCRETE CURB AND GUTTER, TYPE C
- (29) SODDING, NURSERY
- (27) SEED MIXTURE, U
- (B) HMA PAVEMENT
 - 165#/SYD HMA, SURFACE, TYPE C, 9.5 mm, ON
 - 275#/SYD HMA, INTERMEDIATE, TYPE C, 18.0 mm, ON
 - 770#/SYD HMA, BASE, TYPE C, 25.0 mm, ON
 - 6" COMPACTED AGGREGATE, NO. 53, ON
 - SUBGRADE TREATMENT, TYPE AB
- (F) CONCRETE SIDEWALK, 4", ON
 - 4" COMPACTED AGGREGATE, NO. 53
- (G) HMA FOR SIDEWALK
 - 115#/SYD HMA, SURFACE, TYPE B, ON
 - 330#/SYD HMA, INTERMEDIATE, TYPE B, ON
 - 6" COMPACTED AGGREGATE, NO. 53
- (U) UNDERDRAIN, SEE DETAIL, SHEET 65



RECOMMENDED FOR APPROVAL	<i>Don Barth</i>	DATE	10/1/14
DESIGNED:	DJM	DRAWN:	DJM
CHECKED:	DSB	CHECKED:	FS

MONROE COUNTY PROFILE PARKWAY EXTENSION
TYPICAL SECTIONS

HORIZONTAL SCALE	ROAD FILE
N.T.S.	DESIGNATION
VERTICAL SCALE	
N.T.S.	
SURVEY BOOK	SHEETS
ELECTRONIC	4 of 98
CONTRACT	PROJECT
	1763-1207-90

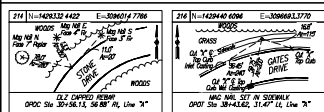
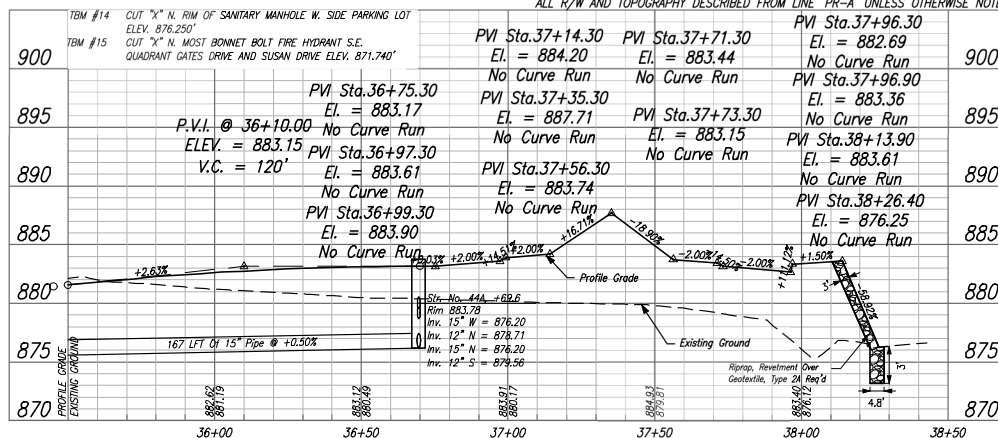


NOTE: WATERMAIN AND SANITARY SEWER IS SHOWN FOR REFERENCE ONLY. SEE SHEETS 21-35 FOR WATER & SANITARY PLANS AND PROFILES.

LEGEND

- (12) ROLLED CURB (SLOPING)
- (13) MODIFIED CURB, CONCRETE
- (14) CONCRETE CURB (VERTICAL)
- (15) COMBINED CONCRETE CURB AND GUTTER, TYPE C
- (22) 7" PCPP, COLORED AND STAMPED (WITH 14" D-1 JOINT SPACING AND 1" DOWEL BARS) ON 13.5" COMPACTED AGGREGATE, NO. 53, ON SUBGRADE TREATMENT, TYPE 1B
- (23) 4" PCPP, COLORED AND STAMPED ON 4" COMPACTED AGGREGATE, NO. 53, ON SODDING, NURSERY
- (27) SEED MIXTURE, U
- (8) HMA PAVEMENT 165#/SYD HMA, SURFACE, TYPE C, 9.5 mm, ON 275#/SYD HMA, INTERMEDIATE, TYPE C, 19.0 mm, ON 770#/SYD HMA, BASE, TYPE C, 25.0 mm, ON 6" COMPACTED AGGREGATE, NO. 53, ON SUBGRADE TREATMENT, TYPE 1B
- (F) CONCRETE SIDEWALK, 4" ON 4" COMPACTED AGGREGATE, NO. 53
- (G) HMA FOR SIDEWALK 110#/SYD HMA, SURFACE, TYPE B, ON 330#/SYD HMA, INTERMEDIATE, TYPE B, ON 6" COMPACTED AGGREGATE, NO. 53
- (H) CHAINLINK FENCE, MODIFIED FOR DETAILS SEE SHEET NO. 3 AND 57.
- (CR) CURB RAMP, FOR DETAILS, SEE SHEET NO. 46 AND 48.

ALL R/W AND TOPOGRAPHY DESCRIBED FROM LINE 'PR-A' UNLESS OTHERWISE NOTED



RECOMMENDED FOR APPROVAL

DESIGN ENGINEER: *David S. Ritz* DATE: *01/10*

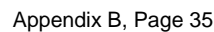
DESIGNED: DJM DRAWN: DPH

CHECKED: DSB CHECKED: DSB

MONROE COUNTY
 PROFILE PARKWAY EXTENSION

PLAN AND PROFILE
 LINE 'PR-A' - PROFILE PARKWAY

HORIZONTAL SCALE 1" = 20'	ROAD FILE
VERTICAL SCALE 1" = 5'	DESIGNATION
SURVEY BOOK ELECTRONIC	SHEETS 13 of 88
CONTRACT	PROJECT 1763-1207-90



APPENDIX C

Early Coordination



Vernal Pike Connector
From West Vernal Pike to Profile Parkway
Des. No.: 1702957 & 1900406
Monroe County, Indiana

Appendix C

January 25, 2019

Monroe County Community School Corporation
315 E. North Drive
Bloomington, IN 47401

Re: Vernal Pike Connector Project
From West Vernal Pike to Profile Parkway
Des. No.: 1702957
Monroe County, Indiana

Sample Early Coordination Request
Note: Project location graphics and
project area photographs have been
removed from this appendix. Similar
graphics are presented in Appendix B
of this Categorical Exclusion document.

Dear Interested Party,

The Federal Highway Administration (FHWA) and Monroe County, Indiana intends to proceed with a project involving the construction of the Vernal Pike Connector from West Vernal Pike to Profile Parkway. DLZ Indiana, LLC is the consultant contracted by Monroe County for the project. Project development activities will require preparation of an environmental study to determine potential project effects upon community, economic and ecological resources.

We are requesting comments per your areas of expertise regarding any possible environmental effects associated with the project. **Please use the above designation number and description in your reply.** We will incorporate your comments into the required study of the project's environmental impacts.

Should a response not be received **within thirty (30) calendar days** from the date of this letter, it will be assumed that your agency feels that there will be no adverse effects incurred as a result of the proposed project. However, should you find that an extension to the response time is necessary; a reasonable amount may be granted upon request.

If you have any questions regarding this matter, please feel free to contact the undersigned by telephone: (574)-236-4400, or email: dstevens@dlz.com. Thank you for your assistance and prompt response to this coordination request.

Very truly yours,



Daniel J. Stevens
Environmental Scientist

cc: INDOT- Office of Environmental Services
DLZ file

The following agencies received Early Coordination letters:

State Conservationist
Natural Resource Conservation Service
Email Early Coordination

Indiana Geological Survey
*IGS Environmental Assessment Website
Coordination*

Manager, Aviation Section, INDOT
Email Early Coordination

Regional Environmental Coordinator
Midwest Regional Office
National Park Service
601 Riverfront Drive
Omaha, NE 68102

Indiana Department of Environmental
Management
Automatic Website Early Coordination

Environmental Coordinator
Indiana Department of Natural Resources
Email Early Coordination

Field Environmental Officer
US Department of Housing and Urban
Development
Email Early Coordination

Manager, Public Hearings
INDOT
Email Early Coordination

Environmental Scoping Manager
INDOT – Seymour District
Email Early Coordination

US Army Corps of Engineers, Louisville District
Email Early Coordination

Bloomington/Monroe County Metropolitan
Planning Organization
Email Early Coordination

Monroe County MS4 Assistant
Email Early Coordination

Monroe County Drainage Engineer
Email Early Coordination

Monroe County Surveyor
Email Early Coordination

Monroe County Sheriff
Email Early Coordination

Northern Monroe Co. Fire Protection Territory
5081 N. Old State Road 37, Bloomington, IN
47408

Monroe County Community School Corporation
315 E. North Drive
Bloomington, IN 47401

Monroe County Commissioners
Email Early Coordination

FHWA Environmental Specialist
Email Early Coordination

USFWS Bloomington Field Office
Email Early Coordination

Forest Supervisor, Hoosier National Forest,
US Forest Service
Email Early Coordination

Monroe County Parks and Recreation
Email Early Coordination

Daniel Stevens

From: Daniel Stevens
Sent: Friday, January 25, 2019 3:42 PM
To: Neilson, Rick - NRCS, Indianapolis, IN; robin_mcwilliams@fws.gov; AFrench2@indot.in.gov; environmentalreview@dnr.in.gov; RCLARK@indot.IN.gov; MWright@indot.in.gov; 'Michael.E.Wurl@hud.gov; martipa@bloomington.in.gov; ddye@indot.in.gov; Gregory.A.McKay@usace.army.mil; dwilkinson@co.monroe.in.us; tqullman@co.monroe.in.us; kamick@fs.fed.us; SurveyorsOffice@co.monroe.in.us; commissionersoffice@co.monroe.in.us; sheriffsoffice@co.monroe.in.us; Antonio.Johnson@dot.gov; mcpr@co.monroe.in.us
Cc: Bales, Ronald; Jason Stone; Drew S. Barth
Subject: Early Coordination Request - Vernal Pike Connector Project - Des. No.: 1702957 - Monroe County, Indiana
Attachments: Project Information - Des 1702957.pdf

Re: Vernal Pike Connector Project
From West Vernal Pike to Profile Parkway
Des. No.: 1702957
Monroe County, Indiana

Sample Early Coordination Email

Dear Interested Party,

The Federal Highway Administration (FHWA) and Monroe County, Indiana intends to proceed with a project involving the construction of the Vernal Pike Connector from West Vernal Pike to Profile Parkway. DLZ Indiana, LLC is the consultant contracted by Monroe County for the project. Project development activities will require preparation of an environmental study to determine potential project effects upon community, economic and ecological resources.

We are requesting comments per your areas of expertise regarding any possible environmental effects associated with the project. **Please use the above designation number and description in your reply.** We will incorporate your comments into the required study of the project's environmental impacts.

Should a response not be received **within thirty (30) calendar days** from the date of this letter, it will be assumed that your agency feels that there will be no adverse effects incurred as a result of the proposed project. However, should you find that an extension to the response time is necessary; a reasonable amount may be granted upon request.

If you have any questions regarding this matter, please feel free to contact the undersigned by telephone: (574)-236-4400, or email: dstevens@dlz.com. Thank you for your assistance and prompt response to this coordination request.

Best Regards,

Daniel Stevens | Environmental Scientist

574-236-4400 x614 (office) | 574-236-4471 (fax) | 574-514-8266 (cell)
dstevens@dlz.com | www.dlz.com



INNOVATIVE IDEAS
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November 21, 2019

Northern Monroe Co. Fire Protection Territory
5081 N. Old State Road 37, Bloomington, IN 47408

Re: Vernal Pike Connector Project
From West Vernal Pike to Profile Parkway
Des. No.: 1702957 and 1900406
Monroe County, Indiana

Sample Additional Early Coordination Request
Note: Project location graphics and project area photographs have been removed from this appendix. Similar graphics are presented in Appendix B of this Categorical Exclusion document.

Dear Interested Party,

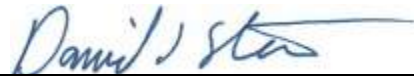
Early coordination was originally submitted for this project on January 25, 2019. Based on comments received, the southern project study limits have been extended approximately 550 feet south to North Gates Drive. This will incorporate a portion of the locally funded Profile Parkway project into the impact assessment for the Vernal Pike Connector Project. This portion of the Profile Parkway project is being included to provide the Vernal Pike Connector project with independent utility in case construction of the locally funded Profile Parkway project becomes delayed.

We are requesting comments per your areas of expertise regarding any possible environmental effects associated with the project. **Please use the above designation number and description in your reply.** We will incorporate your comments into the required study of the project's environmental impacts.

Should a response not be received **within thirty (30) calendar days** from the date of this letter, it will be assumed that your agency feels that there will be no adverse effects incurred as a result of the proposed project. However, should you find that an extension to the response time is necessary; a reasonable amount may be granted upon request.

If you have any questions regarding this matter, please feel free to contact the undersigned by telephone: (574)-236-4400, or email: dstevens@dlz.com. Thank you for your assistance and prompt response to this coordination request.

Very truly yours,



Daniel J. Stevens
Environmental Scientist

cc: INDOT- Office of Environmental Services
DLZ file

The following agencies received Early Coordination letters:

State Conservationist
Natural Resource Conservation Service
Email Early Coordination

Indiana Geological Survey
*IGS Environmental Assessment Website
Coordination*

Manager, Aviation Section, INDOT
Email Early Coordination

Regional Environmental Coordinator
Midwest Regional Office
National Park Service
601 Riverfront Drive
Omaha, NE 68102

Indiana Department of Environmental
Management
Automatic Website Early Coordination

Environmental Coordinator
Indiana Department of Natural Resources
Email Early Coordination

Field Environmental Officer
US Department of Housing and Urban
Development
Email Early Coordination

Manager, Public Hearings
INDOT
Email Early Coordination

Environmental Scoping Manager
INDOT – Seymour District
Email Early Coordination

US Army Corps of Engineers, Louisville District
Email Early Coordination

Bloomington/Monroe County Metropolitan
Planning Organization
Email Early Coordination

Monroe County MS4 Assistant
Email Early Coordination

Monroe County Drainage Engineer
Email Early Coordination

Monroe County Surveyor
Email Early Coordination

Monroe County Sheriff
Email Early Coordination

Northern Monroe Co. Fire Protection Territory
5081 N. Old State Road 37, Bloomington, IN
47408

Monroe County Community School Corporation
315 E. North Drive
Bloomington, IN 47401

Monroe County Commissioners
Email Early Coordination

FHWA Environmental Specialist
Email Early Coordination

USFWS Bloomington Field Office
Email Early Coordination

Forest Supervisor, Hoosier National Forest,
US Forest Service
Email Early Coordination

Monroe County Parks and Recreation
Email Early Coordination

Daniel Stevens

From: Daniel Stevens
Sent: Thursday, November 21, 2019 1:07 PM
To: Neilson, Rick - NRCS, Indianapolis, IN; robin_mcwilliams@fws.gov; AFrench2@indot.in.gov; environmentalreview@dnr.in.gov; RCLARK@indot.IN.gov; MWright@indot.in.gov; Michael.E.Wurl@hud.gov; martipa@bloomington.in.gov; ddye@indot.in.gov; Gregory.A.McKay@usace.army.mil; dwilkinson@co.monroe.in.us; tqullman@co.monroe.in.us; kamick@fs.fed.us; SurveyorsOffice@co.monroe.in.us; commissionersoffice@co.monroe.in.us; sheriffsoffice@co.monroe.in.us; mcpr@co.monroe.in.us; Ruffner, Shelby - NRCS, Indianapolis, IN; erica.tait@dot.gov; JCourtade@indot.in.gov
Cc: Bales, Ronald; Jason Stone; Drew S. Barth; Miller, Brandon
Subject: Additional Early Coordination Request - Vernal Pike Connector Project - Des. No.: 1702957 and 1900406 - Monroe County, Indiana
Attachments: Additional Project Information - Des 1702957 and 1900406.pdf

Re: Vernal Pike Connector Project
From West Vernal Pike to Profile Parkway
Des. No.: 1702957 and 1900406
Monroe County, Indiana

Sample Additional Early Coordination Email

Dear Interested Party,

Early coordination was originally submitted for this project on January 25, 2019. Based on comments received, the southern project study limits have been extended approximately 550 feet south to North Gates Drive. This will incorporate a portion of the locally funded Profile Parkway project into the impact assessment for the Vernal Pike Connector Project. This portion of the Profile Parkway project is being included to provide the Vernal Pike Connector project with independent utility in case construction of the locally funded Profile Parkway project becomes delayed.

We are requesting comments per your areas of expertise regarding any possible environmental effects associated with the modified project. **Please use the above designation number and description in your reply.** We will incorporate your comments into the required study of the project's environmental impacts.

Should a response not be received **within thirty (30) calendar days** from the date of this letter, it will be assumed that your agency feels that there will be no adverse effects incurred as a result of the proposed project. However, should you find that an extension to the response time is necessary; a reasonable amount may be granted upon request.

If you have any questions regarding this matter, please feel free to contact the undersigned by telephone: (574)-236-4400, or email: dstevens@dlz.com. Thank you for your assistance and prompt response to this coordination request.

Best Regards,

Daniel Stevens | Environmental Scientist

574-236-4400 x614 (office) | 574-236-4471 (fax) | 574-514-8266 (cell)
dstevens@dlz.com | www.dlz.com



Indiana Department of Environmental Management

We Protect Hoosiers and Our Environment.

100 North Senate Avenue - Indianapolis, IN 46204
(800) 451-6027 - (317) 232-8603 - www.idem.IN.gov

Monroe County Highway Department
Lisa Ridge
501 N, Morton Street
Bloomington , IN 47404
Date

DLZ Indiana, LLC
Daniel Stevens
2211 E. Jefferson Blvd.
South Bend , IN 46615

To Engineers and Consultants Proposing Roadway Construction Projects:

RE: Monroe County Vernal Pike Connector (Des. No. 1702957 & 1900406). The project includes the extension of Sunrise Greeting Court south to Profile Parkway (which is currently under design), connecting West Vernal Pike to Profile Parkway and North Gates Drive via a railroad overpass. The project will include road and bridge construction on a new alignment over CSX Railroad, with a total project length of approximately 0.5 miles. This project is located in Monroe County, Indiana in Sections 25 and 36, Township 9 North, Range 2 West.

This letter from the Indiana Department of Environmental Management (IDEM) serves as a standardized response to enquiries inviting IDEM comments on roadway construction, reconstruction, or other improvement projects within existing roadway corridors when the proposed scope of the project is beneath the threshold requiring a formal National Environmental Policy Act-mandated Environmental Assessment or Environmental Impact Statement. As the letter attempts to address all roadway-related environmental topics of potential concern, it is possible that not every topic addressed in the letter will be applicable to your particular roadway project.

For additional information on specific roadway-related topics of interest, please visit the appropriate Web pages cited below, many of which provide contact information for persons within the various program areas who can answer questions not fully addressed in this letter. Also please be mindful that some environmental requirements may be subject to change and so each person intending to include a copy of this letter in their project documentation packet is advised to download the most recently revised version of the letter; found at: <http://www.in.gov/idem/5283.htm> (<http://www.in.gov/idem/5283.htm>).

To ensure that all environmentally-related issues are adequately addressed, IDEM recommends that you read this letter in its entirety, and consider each of the following issues as you move forward with the planning of your proposed roadway construction, reconstruction, or improvement project:

WATER AND BIOTIC QUALITY

1. Section 404 of the Clean Water Act requires that you obtain a permit from the U.S. Army Corps of Engineers (USACE) before discharging dredged or fill materials into any wetlands or other waters, such as rivers, lakes, streams, and ditches. Other activities regulated include the relocation, channelization, widening, or other such alteration of a stream, and the mechanical clearing (use of heavy construction equipment) of wetlands. Thus, as a project owner or sponsor, it is your responsibility to ensure that no wetlands are disturbed without the proper permit. Although you may initially refer to the U.S. Fish and Wildlife Service National Wetland Inventory maps as a means of identifying potential areas of concern, please be mindful that those maps do not depict jurisdictional wetlands regulated by the USACE or the Department of Environmental Management. A valid jurisdictional wetlands determination can only be made by the USACE, using the 1987 Wetland Delineation Manual.

USACE recommends that you have a consultant check to determine whether your project will abut, or lie within, a wetland area. To view a list of consultants that have requested to be included on a list posted by the USACE on their Web site, see USACE Permits and Public Notices (<http://www.lrl.usace.army.mil/orf/default.asp>) (<http://www.lrl.usace.army.mil/orf/default.asp>) and then click on "Information" from the menu on the right-hand side of that page. Their "Consultant List" is the fourth entry down on the "Information" page. Please note that the USACE posts all consultants that request to appear on the list, and that inclusion of any particular consultant on the list does not represent an endorsement of that consultant by the USACE, or by IDEM.

Much of northern Indiana (Newton, Lake, Porter, LaPorte, St. Joseph, Elkhart, LaGrange, Steuben, and Dekalb counties; large portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and lesser portions of Benton, White, Pulaski, Kosciusko, and Wells counties) is served by the USACE District Office in Detroit (313-226-6812). The central and southern portions of the state (large portions of Benton, White, Pulaski, Kosciusko, and Wells counties; smaller portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and all other Indiana counties located in north-central, central, and southern Indiana) are served by the USACE Louisville District Office (502-315-6733).

Additional information on contacting these U.S. Army Corps of Engineers (USACE) District Offices, government agencies with jurisdiction over wetlands, and other water quality issues, can be found at <http://www.in.gov/idem/4396.htm> (<http://www.in.gov/idem/4396.htm>). IDEM recommends that impacts to wetlands and other water resources be avoided to the fullest extent.

2. In the event a Section 404 wetlands permit is required from the USACE, you also must obtain a Section 401 Water Quality Certification from the IDEM Office of Water Quality Wetlands Program. To learn more about the Wetlands Program, visit: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>).
3. If the USACE determines that a wetland or other water body is isolated and not subject to Clean Water Act regulation, it is still regulated by the state of Indiana . A State Isolated Wetland permit from IDEM's Office of Water Quality (OWQ) is required for any activity that results in the discharge of dredged or fill materials into isolated wetlands. To learn more about isolated wetlands, contact the OWQ Wetlands Program at 317-233-8488.

4. If your project will involve over a 0.5 acre of wetland impact, stream relocation, or other large-scale alterations to water bodies such as the creation of a dam or a water diversion, you should seek additional input from the OWQ Wetlands Program staff. Consult the Web at: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>) for the appropriate staff contact to further discuss your project.
5. Work within the one-hundred year floodway of a given water body is regulated by the Department of Natural Resources, Division of Water. The Division issues permits for activities regulated under the follow statutes:
 - IC 14-26-2 Lakes Preservation Act 312 IAC 11
 - IC 14-26-5 Lowering of Ten Acre Lakes Act No related code
 - IC 14-28-1 Flood Control Act 310 IAC 6-1
 - IC 14-29-1 Navigable Waterways Act 312 IAC 6
 - IC 14-29-3 Sand and Gravel Permits Act 312 IAC 6
 - IC 14-29-4 Construction of Channels Act No related code

For information on these Indiana (statutory) Code and Indiana Administrative Code citations, see the DNR Web site at: <http://www.in.gov/dnr/water/9451.htm> (<http://www.in.gov/dnr/water/9451.htm>) . Contact the DNR Division of Water at 317-232-4160 for further information.

The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project. The shade provided by the large overhanging trees helps maintain proper stream temperatures and dissolved oxygen for aquatic life.

6. For projects involving construction activity (which includes clearing, grading, excavation and other land disturbing activities) that result in the disturbance of one (1), or more, acres of total land area, contact the Office of Water Quality – Watershed Planning Branch (317/233-1864) regarding the need for of a Rule 5 Storm Water Runoff Permit. Visit the following Web page
 - <http://www.in.gov/idem/4902.htm> (<http://www.in.gov/idem/4902.htm>)

To obtain, and operate under, a Rule 5 permit you will first need to develop a Construction Plan (<http://www.in.gov/idem/4917.htm#constreq> (<http://www.in.gov/idem/4917.htm#constreq>)), and as described in 327 IAC 15-5-6.5 (<http://www.in.gov/legislative/iac/T03270/A00150> [PDF] (<http://www.in.gov/legislative/iac/T03270/A00150.PDF>), pages 16 through 19). Before you may apply for a Rule 5 Permit, or begin construction, you must submit your Construction Plan to your county Soil and Water Conservation District (SWCD) (<http://www.in.gov/isda/soil/contacts/map.html> (<http://www.in.gov/isda/soil/contacts/map.html>)).

Upon receipt of the construction plan, personnel of the SWCD or the Indiana Department of Environmental Management will review the plan to determine if it meets the requirements of 327 IAC 15-5. Plans that are deemed deficient will require re-submittal. If the plan is sufficient you will be notified and instructed to submit the verification to IDEM as part of the Rule 5 Notice of Intent (NOI) submittal. Once construction begins, staff of the SWCD or Indiana Department of Environmental Management will perform inspections of activities at the site for compliance with the regulation.

Please be mindful that approximately 149 Municipal Separate Storm Sewer System (MS4) areas are now being established by various local governmental entities throughout the state as part of the implementation of Phase II federal storm water requirements. All of these MS4 areas will eventually take responsibility for Construction Plan review, inspection, and enforcement. As these MS4 areas obtain program approval from IDEM, they will be added to a list of MS4 areas posted on the IDEM Website at: <http://www.in.gov/idem/4900.htm> (<http://www.in.gov/idem/4900.htm>).

If your project is located in an IDEM-approved MS4 area, please contact the local MS4 program about meeting their storm water requirements. Once the MS4 approves the plan, the NOI can be submitted to IDEM.

Regardless of the size of your project, or which agency you work with to meet storm water requirements, IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff. The use of appropriate planning and site development and appropriate storm water quality measures are recommended to prevent soil from leaving the construction site during active land disturbance and for post construction water quality concerns. Information and assistance regarding storm water related to construction activities are available from the Soil and Water Conservation District (SWCD) offices in each county or from IDEM.

7. For projects involving impacts to fish and botanical resources, contact the Department of Natural Resources - Division of Fish and Wildlife (317/232-4080) for addition project input.
8. For projects involving water main construction, water main extensions, and new public water supplies, contact the Office of Water Quality - Drinking Water Branch (317-308-3299) regarding the need for permits.
9. For projects involving effluent discharges to waters of the State of Indiana , contact the Office of Water Quality - Permits Branch (317-233-0468) regarding the need for a National Pollutant Discharge Elimination System (NPDES) permit.
10. For projects involving the construction of wastewater facilities and sewer lines, contact the Office of Water Quality - Permits Branch (317-232-8675) regarding the need for permits.

AIR QUALITY

The above-noted project should be designed to minimize any impact on ambient air quality in, or near, the project area. The project must comply with all federal and state air pollution regulations.

Consideration should be given to the following:

1. Regarding open burning, and disposing of organic debris generated by land clearing activities; some types of open burning are allowed (<http://www.in.gov/idem/4148.htm> (<http://www.in.gov/idem/4148.htm>)) under specific conditions. You also can seek an open burning variance from IDEM.

However, IDEM generally recommends that you take vegetative wastes to a registered yard waste composting facility or that the waste be chipped or shredded with composting on site (you

must register with IDEM if more than 2,000 pounds is to be composted; contact 317/232-0066). The finished compost can then be used as a mulch or soil amendment. You also may bury any vegetative wastes (such as leaves, twigs, branches, limbs, tree trunks and stumps) onsite, although burying large quantities of such material can lead to subsidence problems, later on.

Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. For example, wetting the area with water, constructing wind barriers, or treating dusty areas with chemical stabilizers (such as calcium chloride or several other commercial products). Dirt tracked onto paved roads from unpaved areas should be minimized.

Additionally, if construction or demolition is conducted in a wooded area where blackbirds have roosted or abandoned buildings or building sections in which pigeons or bats have roosted for 3-5 years precautionary measures should be taken to avoid an outbreak of histoplasmosis. This disease is caused by the fungus *Histoplasma capsulatum*, which stems from bird or bat droppings that have accumulated in one area for 3-5 years. The spores from this fungus become airborne when the area is disturbed and can cause infections over an entire community downwind of the site. The area should be wetted down prior to cleanup or demolition of the project site. For more detailed information on histoplasmosis prevention and control, please contact the Acute Disease Control Division of the Indiana State Department of Health at (317) 233-7272.

2. The U.S. EPA and the Surgeon General recommend that people not have long-term exposure to radon at levels above 4 pCi/L. (For a county-by-county map of predicted radon levels in Indiana, visit: <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>).)

The U.S. EPA further recommends that all homes (and apartments within three stories of ground level) be tested for radon. If in-home radon levels are determined to be 4 pCi/L, or higher, EPA recommends a follow-up test. If the second test confirms that radon levels are 4 pCi/L, or higher, EPA recommends the installation of radon-reduction measures. (For a list of qualified radon testers and radon mitigation (or reduction) specialists visit:

http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf

(http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf).) It also is recommended that radon reduction measures be built into all new homes, particularly in areas like Indiana that have moderate to high predicted radon levels.

To learn more about radon, radon risks, and ways to reduce exposure visit:

<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>

(<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>), <http://www.in.gov/idem/4145.htm>

(<http://www.in.gov/idem/4145.htm>), or <http://www.epa.gov/radon/index.html>

(<http://www.epa.gov/radon/index.html>).

3. With respect to asbestos removal: all facilities slated for renovation or demolition (except residential buildings that have (4) four or fewer dwelling units and which will not be used for commercial purposes) must be inspected by an Indiana-licensed asbestos inspector prior to the commencement of any renovation or demolition activities. If regulated asbestos-containing material (RACM) that may become airborne is found, any subsequent demolition, renovation, or

asbestos removal activities must be performed in accordance with the proper notification and emission control requirements.

If no asbestos is found where a renovation activity will occur, or if the renovation involves removal of less than 260 linear feet of RACM off of pipes, less than 160 square feet of RACM off of other facility components, or less than 35 cubic feet of RACM off of all facility components, the owner or operator of the project does not need to notify IDEM before beginning the renovation activity.

For questions on asbestos demolition and renovation activities, you can also call IDEM's Lead/Asbestos section at 1-888-574-8150.

However, in all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition, using the form found at <http://www.in.gov/icpr/webfile/formsdiv/44593.pdf> (<http://www.in.gov/icpr/webfile/formsdiv/44593.pdf>).

Anyone submitting a renovation/demolition notification form will be billed a notification fee based upon the amount of friable asbestos containing material to be removed or demolished. Projects that involve the removal of more than 2,600 linear feet of friable asbestos containing materials on pipes, or 1,600 square feet or 400 cubic feet of friable asbestos containing material on other facility components, will be billed a fee of \$150 per project; projects below these amounts will be billed a fee of \$50 per project. All notification remitters will be billed on a quarterly basis.

For more information about IDEM policy regarding asbestos removal and disposal, visit: <http://www.in.gov/idem/4983.htm> (<http://www.in.gov/idem/4983.htm>).

4. With respect to lead-based paint removal: IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust. IDEM is particularly concerned that young children exposed to lead can suffer from learning disabilities. Although lead-based paint abatement efforts are not mandatory, any abatement that is conducted within housing built before January 1, 1978 , or a child-occupied facility is required to comply with all lead-based paint work practice standards, licensing and notification requirements. For more information about lead-based paint removal visit: <http://www.in.gov/isdh/19131.htm> (<http://www.in.gov/isdh/19131.htm>).
5. Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7%) oil distillate, is prohibited during the months April through October. See 326 IAC 8-5-2 , Asphalt Paving Rule (<http://www.ai.org/legislative/iac/T03260/A00080.PDF> (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>)).
6. If your project involves the construction of a new source of air emissions or the modification of an existing source of air emissions or air pollution control equipment, it will need to be reviewed by the IDEM Office of Air Quality (OAQ). A registration or permit may be required under 326 IAC 2 (View at: www.ai.org/legislative/iac/t03260/a00020.pdf (<http://www.ai.org/legislative/iac/t03260/a00020.pdf>)). New sources that use or emit hazardous air pollutants may be subject to Section 112 of the Clean Air Act and corresponding state air regulations governing hazardous air pollutants.

7. For more information on air permits visit: <http://www.in.gov/idem/4223.htm> (<http://www.in.gov/idem/4223.htm>), or to initiate the IDEM air permitting process, please contact the Office of Air Quality Permit Reviewer of the Day at (317) 233-0178 or OAMPROD at adem.state.in.us.

LAND QUALITY

In order to maintain compliance with all applicable laws regarding contamination and/or proper waste disposal, IDEM recommends that:

1. If the site is found to contain any areas used to dispose of solid or hazardous waste, you need to contact the Office of Land Quality (OLQ) at 317-308-3103.
2. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. For more information, visit <http://www.in.gov/idem/4998.htm> (<http://www.in.gov/idem/4998.htm>).
3. If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. Please contact the OLQ at 317-308-3103 to obtain information on proper disposal procedures.
4. If PCBs are found at this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding management of any PCB wastes from this site.
5. If there are any asbestos disposal issues related to this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding the management of asbestos wastes (Asbestos removal is addressed above, under Air Quality).
6. If the project involves the installation or removal of an underground storage tank, or involves contamination from an underground storage tank, you must contact the IDEM Underground Storage Tank program at 317/308-3039. See: <http://www.in.gov/idem/4999.htm> (<http://www.in.gov/idem/4999.htm>).

FINAL REMARKS

Should you need to obtain any environmental permits in association with this proposed project, please be mindful that IC 13-15-8 requires that you notify all adjoining property owners and/or occupants within ten days your submittal of each permit application. However, if you are seeking multiple permits, you can still meet the notification requirement with a single notice if all required permit applications are submitted with the same ten day period.

Should the scope of the proposed project be expanded to the extent that a National Environmental Policy Act Environmental Assessment (EA) or Environmental Impact Statement (EIS) is required, IDEM will actively participate in any early interagency coordination review of the project.

Meanwhile, please note that this letter does not constitute a permit, license, endorsement or any other form of approval on the part of the Indiana Department of Environmental Management regarding any project for which a copy of this letter is used. Also note that it is the responsibility of the project engineer or consultant using this letter to ensure that the most current draft of this document, which is located at <http://www.in.gov/idem/5284.htm> (<http://www.in.gov/idem/5284.htm>), is used.

Signature(s) of the Applicant

I acknowledge that the following proposed roadway project will be financed in part, or in whole, by public monies.

Project Description

Monroe County Vernal Pike Connector (Des. No. 1702957 & 1900406). The project includes the extension of Sunrise Greeting Court south to Profile Parkway (which is currently under design), connecting West Vernal Pike to Profile Parkway and North Gates Drive via a railroad overpass. The project will include road and bridge construction on a new alignment over CSX Railroad, with a total project length of approximately 0.5 miles. This project is located in Monroe County, Indiana in Sections 25 and 36, Township 9 North, Range 2 West.

With my signature, I do hereby affirm that I have read the letter from the Indiana Department of Environment that appears directly above. In addition, I understand that in order to complete that project in which I am interested, with a minimum of impact to the environment, I must consider all the issues addressed in the aforementioned letter, and further, that I must obtain any required permits.

Date: 4-13-2020

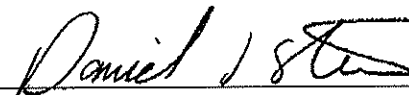
Signature of the INDOT
Project Engineer or Other Responsible Agent



Lisa Ridge

Date: 4-13-2020

Signature of the
For Hire Consultant



Daniel Stevens

Daniel Stevens

From: JOHANSON, SCOTT <SJOHANSO@idem.IN.gov>
Sent: Thursday, February 28, 2019 2:44 PM
To: Daniel Stevens
Subject: RE: Early Coordination Request - Vernal Pike Connector Project - Des. No.: 1702957 - Monroe County, Indiana

February 28, 2019

Vernal Pike Connector Project – Des. No.: 1702957
From West Vernal Pike to Profile Parkway
Bloomington, Monroe County

Daniel Stevens,

Please find my comments for the referenced document prepared by DLZ.

After reviewing the Early Coordination Project Information provided in the February 26, 2019 E-mail from DLZ, it appears the new road will extend across a portion of the Former ABB plant. Given the known environmental conditions and current property restrictions, DLZ needs to coordinate with Jessica Fliss (IDEM at JFLISS@idem.IN.gov), Thomas Alcamo (USEPA at alcamo.thomas@epa.gov), for further technical evaluation and guidance prior to proceeding with the new road.

Let me know if you have any additional questions or concerns.

Scott Johanson, LPG # IN 1813
Geological Services
Science Services Branch
Office of Land Quality
Indiana Department of Environmental Management
(317) 234-0996 FAX: (317) 234-0428
(800) 451-6027
sjohanso@idem.IN.gov

This document reflects the opinions of technical staff based on information presented in the report under review addressing the condition of the site, including other relevant information available at the time of the investigation. It is intended for use in agency decision making and does not contain final determinations regarding potential remedial actions. Information in subsequent tech memos may diverge from information contained in this document based on changing site conditions or the discovery of additional relevant information.

From: Daniel Stevens [mailto:dstevens@dlz.com]
Sent: Tuesday, February 26, 2019 1:27 PM
To: JOHANSON, SCOTT <SJOHANSO@idem.IN.gov>
Cc: SULLIVAN, JAMES <JSULLIVA@idem.IN.gov>; Drew S. Barth <dsbarth@dlz.com>; Jason Stone <jstone@dlz.com>; Haseeb A. Ghumman, PE, PTOE <hghumman@dlz.com>
Subject: Early Coordination Request - Vernal Pike Connector Project - Des. No.: 1702957 - Monroe County, Indiana

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Scott Johanson, LPG # IN 1813
Geological Services
Science Services Branch
Office of Land Quality
Indiana Department of Environmental Management

Dear Mr. Johanson

I received an email from USFWS that suggested I include you in our early coordination. Please let me know if you have any comments relating to the project based on the email invitation below or attached project information.

Thanks,

Daniel Stevens | Environmental Scientist

574-236-4400 x614 (office) | 574-236-4471 (fax) | 574-514-8266 (cell)
dstevens@dlz.com | www.dlz.com



From: Daniel Stevens

Sent: Friday, January 25, 2019 3:42 PM

To: Neilson, Rick - NRCS, Indianapolis, IN <rick.neilson@in.usda.gov>; robin_mcwilliams@fws.gov;
AFrench2@indot.in.gov; environmentalreview@dnr.in.gov; RCLARK@indot.IN.gov; MWright@indot.in.gov;
'Michael.E.Wurl@hud.gov; martipa@bloomington.in.gov; ddye@indot.in.gov; Gregory.A.McKay@usace.army.mil;
dwilkinson@co.monroe.in.us; tquillman@co.monroe.in.us; kamick@fs.fed.us; SurveyorsOffice@co.monroe.in.us;
commissionersoffice@co.monroe.in.us; sheriffsoffice@co.monroe.in.us; Antonio.Johnson@dot.gov;
mcpr@co.monroe.in.us

Cc: Bales, Ronald <rbales@indot.IN.gov>; Jason Stone <jstone@dlz.com>; Drew S. Barth <dsbarth@dlz.com>

Subject: Early Coordination Request - Vernal Pike Connector Project - Des. No.: 1702957 - Monroe County, Indiana

Re: Vernal Pike Connector Project
From West Vernal Pike to Profile Parkway
Des. No.: 1702957
Monroe County, Indiana

Dear Interested Party,

The Federal Highway Administration (FHWA) and Monroe County, Indiana intends to proceed with a project involving the construction of the Vernal Pike Connector from West Vernal Pike to Profile Parkway. DLZ Indiana, LLC is the consultant contracted by Monroe County for the project. Project development activities will require preparation of an environmental study to determine potential project effects upon community, economic and ecological resources.

Organization and Project Information

Project ID:
Des. ID: 1702957 and 1900406
Project Title: Vernal Pike Connector
Name of Organization: DLZ Indiana, LLC
Requested by: Daniel Stevens

Environmental Assessment Report

1. Geological Hazards:
 - Potential Karst
 - High liquefaction potential
2. Mineral Resources:
 - Bedrock Resource: Low Potential
 - Sand and Gravel Resource: Low Potential
3. Active or abandoned mineral resources extraction sites:
 - None documented in the area

*All map layers from Indiana Map (maps.indiana.edu)

DISCLAIMER:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

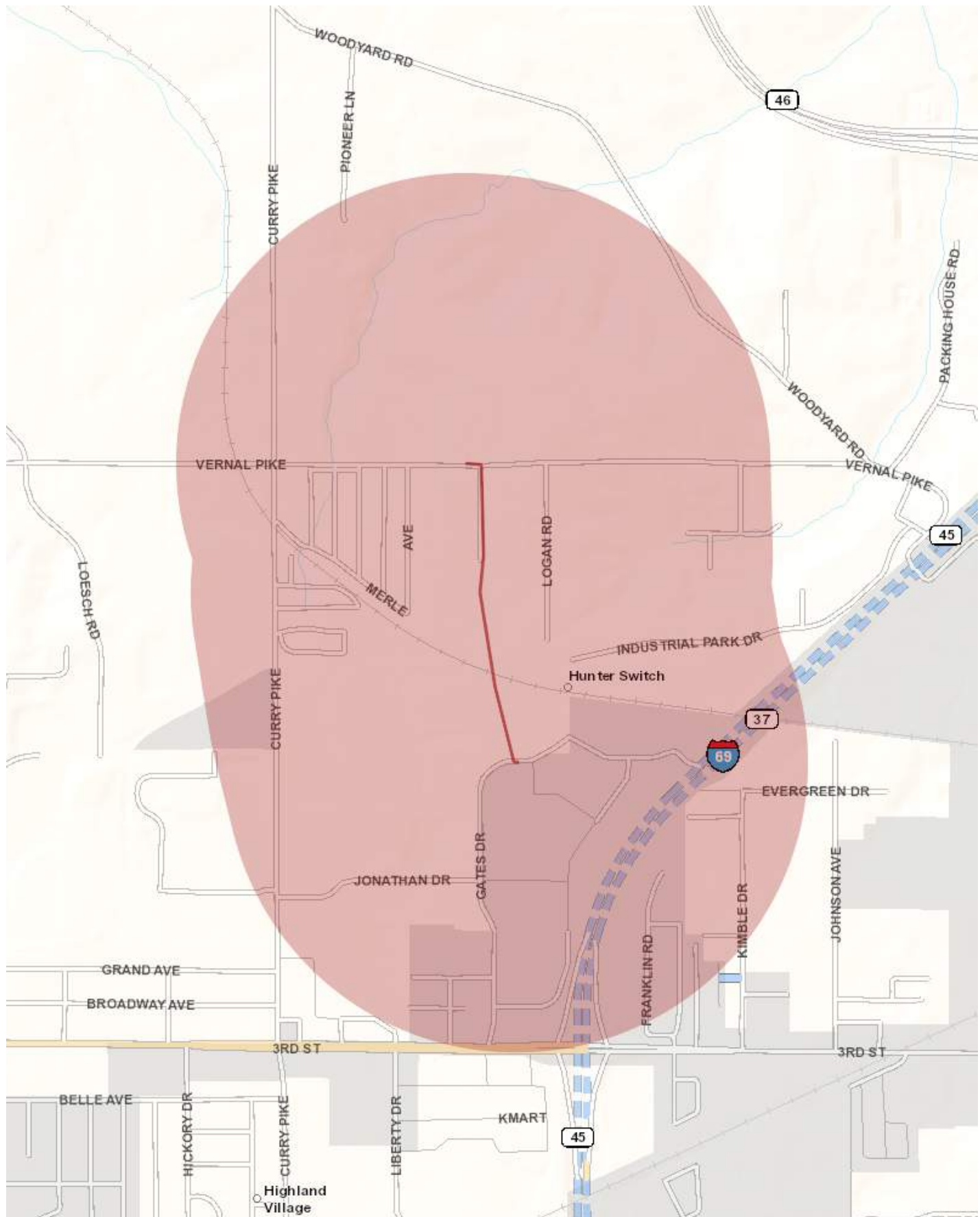
This information was furnished by Indiana Geological Survey

Address: 420 N. Walnut St., Bloomington, IN 47404

Email: IGSEnvir@indiana.edu

Phone: 812 855-7428

Date: February 11, 2020



Metadata:

- https://maps.indiana.edu/metadata/Hydrology/Karst_Cave_Density.html
- https://maps.indiana.edu/metadata/Hydrology/Karst_Sinkhole_Areas.html
- https://maps.indiana.edu/metadata/Hydrology/Karst_Springs.html
- https://maps.indiana.edu/metadata/Geology/Seismic_Earthquake_Liquefaction_Potential.html
- https://maps.indiana.edu/metadata/Geology/Industrial_Minerals_Sand_Gravel_Resources.html
- https://maps.indiana.edu/metadata/Geology/Bedrock_Geology.html

Daniel Stevens

From: Patrick Martin <martipa@bloomington.in.gov>
Sent: Thursday, February 14, 2019 2:36 PM
To: Daniel Stevens
Cc: Ryan Clemens; Beth Rosenbarger
Subject: Re: Early Coordination Request - Vernal Pike Connector Project - Des. No.: 1702957 - Monroe County, Indiana

Good Afternoon Daniel,

Thank you for your Early Coordination letter for the Vernal Pike Connector Project (DES#1702957) extending from West Vernal Pike to Profile Parkway.

The proposed Vernal Pike connector project is consistent with the Bloomington-Monroe County Metropolitan Planning Organization (BMCMPPO) 2040 Metropolitan Transportation Plan, the 2018 Complete Streets Policy, and the Monroe County Thoroughfare Plan.

The BMCMPPO fully supports development of the proposed project with its ability to interconnect roadways serving a commercial and industrial area that lost two access points with the construction of the I-69/SR37 corridor through Bloomington and Monroe County.

Please contact us at your earliest convenience if you should have any questions or require any additional information.

Thanks,

Pat Martin

On Fri, Jan 25, 2019 at 3:42 PM Daniel Stevens <dstevens@dlz.com> wrote:

Re: Vernal Pike Connector Project

From West Vernal Pike to Profile Parkway

Des. No.: 1702957

Monroe County, Indiana

Dear Interested Party,



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N955
Indianapolis, Indiana 46204

PHONE: (317) 232-1477
FAX: (317) 232-1499

Eric Holcomb, Governor
Joe McGuinness, Commissioner

February 4, 2019

Mr. Daniel Stevens, Environmental Scientist
DLZ
2211 East Jefferson Blvd
South Bend, IN 46615

Subject: Early Coordination Review (Des. No. 1702957)

Dear Mr. Stevens,

In response to your request received on January 25, 2019 for early coordination review of the Vernal Pike Connector project in Monroe County, Indiana; the Indiana Department of Transportation, Office of Aviation has reviewed the information and provides the following:

Are there any existing or proposed public-use airports within 5 nautical miles of the project limits (IC 8-21-10-6)?

The Monroe County Airport is located approximately 2.1 nautical miles northeast of the proposed project corridor.

Will an Indiana Tall Structure permit (IC 8-21-10-3-a) and/or Noise Sensitive (IC 8-21-10-3-b) permit be required?

Based upon the provided information, an Indiana Tall Structure permit may be required if the project involves the construction of a temporary (e.g., crane) or permanent structure that penetrates a 100:1 slope from the nearest point of the Monroe County Airport runways.

For any questions related to Indiana Tall Structure and/or Noise Sensitive permitting, please contact James Kinder at (317) 232-1485 or jkinder2@indot.in.gov.

Sincerely,

Adam French, MPA
Chief Airport Inspector, Office of Aviation
Indiana Department of Transportation

Daniel Stevens

From: Courtade, Julian <JCourtade@indot.IN.gov>
Sent: Tuesday, November 26, 2019 8:13 AM
To: Daniel Stevens
Subject: RE: Additional Early Coordination Request - Vernal Pike Connector Project - Des. No.: 1702957 and 1900406 - Monroe County, Indiana

EXTERNAL: Message origin is from an external network. Use proper judgment and caution when opening attachments, clicking links, or responding to this email.

Hello –

After reviewing the ECL I have determined that if any object, obstruction, or equipment will exceed 75 ft in height, further coordination will be required with our office. This is because of the close proximity of Monroe County Airport. Please let me know if you have any questions!

Best,

Julian L. Courtade
Chief Airport Inspector
INDOT, Office of Aviation
IGCN Room N955
100 North Senate Avenue
Indianapolis, IN 46204
Office: (317) 232-1477
Email: jcourtade@indot.in.gov



From: Daniel Stevens [mailto:dsteven@dlz.com]
Sent: Thursday, November 21, 2019 1:07 PM
To: Neilson, Rick - NRCS, Indianapolis, IN <rick.neilson@in.usda.gov>; robin_mcwilliams@fws.gov; Courtade, Julian <JCourtade@indot.IN.gov>; DNR Environmental Review <environmentalreview@dnr.IN.gov>; Clark, Rickie <RCLARK@indot.IN.gov>; Wright, Mary <MWRIGHT@indot.IN.gov>; Michael.E.Wurl@hud.gov; martipa@bloomington.in.gov; Dye, David <DDYE@indot.IN.gov>; Gregory.A.McKay@usace.army.mil; dwilkinson@co.monroe.in.us; tqullman@co.monroe.in.us; kamick@fs.fed.us; SurveyorsOffice@co.monroe.in.us; commissionersoffice@co.monroe.in.us; sheriffsoffice@co.monroe.in.us; mcpr@co.monroe.in.us; Ruffner, Shelby - NRCS, Indianapolis, IN <Shelby.Ruffner@in.usda.gov>; erica.tait@dot.gov; Courtade, Julian <JCourtade@indot.IN.gov>
Cc: Bales, Ronald <rbales@indot.IN.gov>; Jason Stone <jstone@dlz.com>; Drew S. Barth <dsbarth@dlz.com>; Miller, Brandon <BrMiller1@indot.IN.gov>
Subject: Additional Early Coordination Request - Vernal Pike Connector Project - Des. No.: 1702957 and 1900406 - Monroe County, Indiana

Daniel Stevens

From: Wright, Mary <MWRIGHT@indot.IN.gov>
Sent: Tuesday, January 29, 2019 7:09 AM
To: Daniel Stevens
Subject: RE: Early Coordination Request - Vernal Pike Connector Project - Des. No.: 1702957 - Monroe County, Indiana

Early Coordination and Creating a Public Involvement Plan (PIP)

We have received your early coordination notification packet for the above referenced project(s). Our office prefers to be notified at the early coordination stage in order to encourage early and ongoing public involvement aside from the specific legal requirements as outlined in our Public Involvement Manual <http://www.in.gov/indot/2366.htm>. Seeking the public's understanding of transportation improvement projects early in the project development stage can allow the opportunity for the public to express their concerns, comments, and to seek buy-in. Early coordination is the perfect opportunity to examine the proposed project and its impacts to the community along with the many ways and or tools to inform the public of the improvements and seek engagement. A good public involvement plan, or PIP, should consider the type, scope, impacts, and the level of public awareness that should, or could, be implemented. In other words, although there are cases where no public involvement is legally required, sometimes it is simply the right thing to do in order to keep the public informed.

The public involvement office is always available to provide support and resources to bolster any public involvement activities you may wish to implement or discuss. Please feel free to contact our office anytime should you have any questions or concerns. Thank you for notifying our office about your proposed project. We trust you will not only analyze the appropriate public involvement required, but also consider the opportunity to do go above and beyond those requirements in creating a good PIP.

Rickie Clark, Manager
100 North Senate Avenue, Room N642
Indianapolis, IN 46204
Phone: 317-232-6601
Email: rclark@indot.in.gov

Mary Wright, Hearing Examiner
Phone: 317-234-0796
Email: mwright@indot.in.gov

From: Daniel Stevens [mailto:dstevens@dlz.com]

Sent: Friday, January 25, 2019 3:42 PM

To: Neilson, Rick - NRCS, Indianapolis, IN <rick.neilson@in.usda.gov>; robin_mcwilliams@fws.gov; French, Adam <AFrench2@indot.IN.gov>; DNR Environmental Review <environmentalreview@dnr.IN.gov>; Clark, Rickie <RCLARK@indot.IN.gov>; Wright, Mary <MWRIGHT@indot.IN.gov>; 'Michael.E.Wurl@hud.gov'; martipa@bloomington.in.gov; Dye, David <DDYE@indot.IN.gov>; Gregory.A.McKay@usace.army.mil; dwilkinson@co.monroe.in.us; tquillman@co.monroe.in.us; kamick@fs.fed.us; SurveyorsOffice@co.monroe.in.us; commissionersoffice@co.monroe.in.us; sheriffsoffice@co.monroe.in.us; Antonio.Johnson@dot.gov; mcpr@co.monroe.in.us

Cc: Bales, Ronald <rbales@indot.IN.gov>; Jason Stone <jstone@dlz.com>; Drew S. Barth <dsbarth@dlz.com>

Subject: Early Coordination Request - Vernal Pike Connector Project - Des. No.: 1702957 - Monroe County, Indiana

Re: Vernal Pike Connector Project

March 5, 2019

Daniel Stevens
DLZ
2211 East Jefferson Boulevard
South Bend, Indiana 46615

Dear Mr. Stevens:

The proposed project to extend Sunrise Greeting Court south to Profile Parkway, connecting West Vernal Parkway to Profile Parkway via a railroad overpass in Monroe County, Indiana, (Des No 1702957), as stated in your letter received January 25, 2019, will not cause a conversion of prime farmland.

If you need additional information, please contact Daniel Phillips at 317-295-5871.

Sincerely,

JERRY RAYNOR
Digitally signed by JERRY
RAYNOR
Date: 2019.03.07 10:28:14
-05'00'

JERRY RAYNOR
State Conservationist



November 27, 2019

Jason A. Stone
DLZ
2211 East Jefferson Boulevard
South Bend, Indiana 46615

Dear Mr. Stone:

The proposed project to provide a connection between the existing commercial and industrial developments in the project vicinity in Monroe County, Indiana, (Des No. 1702957 & 1900406) as referred to in your letter received November 21, 2019, will not cause a conversion of prime farmland.

If you need additional information, please contact John Allen at 317-295-5859.

Sincerely,

JERRY RAYNOR Digitally signed by JERRY
RAYNOR
Date: 2019.11.29 17:04:40 -05'00'

JERRY RAYNOR
State Conservationist



State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR #: ER-21179

Request Received: January 25, 2019

Requestor: DLZ Indiana, LLC
Daniel J Stevens
2211 East Jefferson Boulevard
South Bend, IN 46615-2607

Project: Vernal Pike Connector Project: Sunrise Greeting Court extension of about 0.45 mile from Vernal Pike to Profile Parkway, and construction of new overpass over CSX railroad, Bloomington; Des #1702957

County/Site info: Monroe

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment: Formal approval by the Department of Natural Resources under the regulatory programs administered by the Division of Water is not required for this project.

Natural Heritage Database: The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Fish & Wildlife Comments: Avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. The following are recommendations that address potential impacts identified in the proposed project area:

A) Riparian & Forest Habitat:

This project is proposed to cut partially through undisturbed habitat and will likely result in significant environmental impacts. The placement of a multi-use trail on the east side of the new road will widen the right-of-way needs and natural resource impacts on the side of the road containing a forested area about seven (7) acres in size. To minimize impacts to this forested habitat, the Division of Fish and Wildlife recommends placing the trail on the opposite side of the road away from the forested area or shifting the entire project in such a way that avoids the forested habitat to the extent possible and use the narrowest width possible for the trail.

We recommend a mitigation plan be developed for any unavoidable habitat impacts that will occur. The mitigation site should be located preferably as close to the impact site as possible and adjacent to existing forested habitat. The DNR's Floodway Habitat Mitigation guidelines (and plant lists) can be found online at:
<http://www.in.gov/legislative/iac/20190130-IR-312190041NRA.xml.pdf>.

Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to non-wetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10" dbh or greater (5:1 mitigation based on the number of large trees). Impacts to wetland habitat should be mitigated at the appropriate ratio according to the 1991

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INDOT/IDNR/USFWS Memorandum of Understanding.

Mitigation at a ratio of 1:1 would be needed for a permanent impact to early successional riparian habitat. A native herbaceous riparian seed mixture is planted with at least 10 species of native grasses, sedges, and wildflowers selected from the Herbaceous Riparian Vegetation List in Appendix A. If the area contains scattered shrubs or tree saplings, mitigation includes woody species native to the region.

B) Karst Features:

The Bloomington area is generally characterized by underlying karst geology; therefore, we recommend implementation of the 1993 INDOT/IDNR/IDEM/USFWS Karst Memorandum of Understanding during all phases of the project (http://www.in.gov/indot/files/38_karst.pdf).

Should any karst features be located within the construction limits or that may receive drainage from the construction, a karst assessment should be conducted by a qualified geologist and a determination made as to whether or not the karst feature/sinkhole is active. If a karst assessment is not done, any sinkhole that construction runoff may drain to should be assumed to be active. To protect active sinkholes (or those not assessed), the most protective erosion control methods should be implemented to avoid potentially impacting sensitive karst ecosystems (such as runoff containment and filtering prior to discharge).

Construction should be avoided within 25' of the topmost closed contour of any active karst features. Where construction within the closed contours of a karst feature is unavoidable, runoff must be filtered prior to discharge.

C) Lighting:

The International Dark-Sky Association (IDA) states that, to minimize the negative impacts of artificial lighting on wildlife, "lighting should only be on when needed, only light the area that needs it, be no brighter than necessary, minimize blue light emissions, [and] be fully shielded (pointing downward)". The Division of Fish and Wildlife strongly encourages visiting the IDA's website to learn more about selecting lighting fixtures that minimize the harmful effects of lighting on humans and wildlife: <http://darksky.org/lighting/lighting-basics/>.

D) Trail Guidelines:

The following is a basic list of recommendations from IDNR Division of Fish and Wildlife to consider when planning trails to minimize impacts to fish, wildlife, and botanical resources (these guidelines can help aid development of the road plans as well).

1. Place the trail in or adjacent to existing right-of-ways where possible to minimize significant impacts to natural resource habitat. Also, utilize previously disturbed or degraded areas. Align the trail along or near existing man-made edges or areas that have the potential to be restored or enhanced by trail construction (i.e. railroad corridors), rather than routing the trail through previously undisturbed areas.
2. When designing or constructing a trail, disturb as narrow an area as possible to help minimize negative impacts. Where significant impacts to fish, wildlife or botanical resources are likely due to the trail's width, reduce the width to help avoid those impacts. ADA accessibility standards allow departures from the standards under certain conditions, including substantial harm to natural features, habitat, or vegetation (see <http://www.access-board.gov/attachments/article/1500/outdoor-rule.pdf>, Accessibility Guidelines for Outdoor Developed Areas).
3. Do not focus only on the direct impact of the trail's width; also consider the trail's impact to the surrounding habitat. Trails can fragment larger habitat areas and reduce the overall usefulness of the site to fish, wildlife, or botanical resources (1 large habitat

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block is better than 2 small habitat blocks). Trails can cause significant impacts to forested areas, riparian forested corridors along creeks and rivers, and wetland areas. They also may cause sediment and erosion issues or introduce human disturbance into fairly isolated areas containing wildlife habitat.

4. Avoid unnecessary stream crossings. Instead, make use of or modify existing stream crossings or avoid crossing the stream altogether. Where stream crossings are unavoidable, pedestrian bridges with supports/abutments placed no less than 10 feet landward from the tops of the banks on each side of the waterway are recommended. Alternatively, a three-sided culvert may be used. Three-sided culverts should be oversized to allow terrestrial wildlife movement along the creek on unsubmerged dry land at normal water levels. Box-culvert or pipe-culvert crossings are not recommended.

5. Trails designed to follow a stream's course must be placed outside the stream's forested riparian buffer. Also, do not place the trail along the tops of the banks of a forested creek. Avoid perpendicular fragmentation of riparian areas (streamside habitat). Where the stream has little or no forested riparian buffer, the trail should be no closer than 15 feet from the tops of the banks.

6. Avoid elements identified in the Natural Heritage Database; trails may negatively affect species that require specific natural conditions (vegetation, light levels, moisture, etc.) that are altered as a result of trail construction. Rare and high quality habitats, and wildlife habitats that possess high wildlife abundance and diversity, should be avoided by placing the trail around the habitat and screening it from the trail and trail users with a buffer of native vegetation or another method as discussed below. Wetlands and karst features are but two examples of areas to avoid.

7. Raised boardwalks should be constructed in wet areas or near wetlands (trails through wetlands are not recommended). A material such as composite decking should be used rather than treated wood which can leach elements toxic to aquatic life.

8. Screen wildlife habitat from the trail corridor. Vegetation, topography, and fences can help reduce the impact of noise and line of sight disturbances of trail users on wildlife. Walls can create wildlife movement barriers and potential impacts must be considered. Native grass buffers (2 to 3 feet tall) are recommended along the edge of trails near habitat such as wetlands.

9. Lighting should only be used when absolutely necessary. Lighting in forested areas and along creeks, streams, and rivers should be the lowest intensity feasible and shielded to cast light on the path and not diffused into the surroundings to avoid disturbing wildlife circadian rhythms and disorienting night-migrating birds.

10. Any plantings in the riparian areas should be locally native species, not exotic species or horticultural varieties (e.g. "Autumn Blaze" Red Maple). A list of appropriate native woody and herbaceous vegetation can be provided upon request.

11. Trail surfaces can have negative effects on surrounding natural areas and deter movement of some species across the trail. Some surface materials are more environmentally acceptable than others, such as mulch and mown grass which should be considered as the first options. Asphalt is not recommended as a trail surface in the floodway. The conventional maintenance for aging asphalt is to seal it with a blacktop or asphalt sealer. Research has shown that as these sealers break down over time, they move into the aquatic environment and are highly toxic to aquatic life. If asphalt is used then asphalt sealer should not be used for long-term maintenance and repair of the asphalt trail surface. In previously disturbed areas, concrete is an acceptable surface material, and porous concrete is preferred wherever it can be used.

12. Shoulders should be constructed using unconsolidated materials where possible. In some situations, solid shoulders are necessary. In those cases, shoulders should be constructed using porous concrete.

13. Trails that highlight natural resources should skirt the resource and utilize "pulloffs" at specific sites instead of letting the entire trail and traffic disturb the resource.

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The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas in the floodway with a mixture of native grasses, sedges, wildflowers, and also native hardwood trees and shrubs if any woody plants are disturbed during construction as soon as possible upon completion. Do not use any varieties of Tall Fescue or other non-native plants, including prohibited invasive species (see 312 IAC 18-3-25).
2. Minimize and contain within the project limits all tree and brush clearing.
3. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (greater than 3 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
4. Post "Do Not Mow or Spray" signs along the right-of-way.
5. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
6. Seed and protect all disturbed streambanks and slopes not protected by other methods that are 3:1 or steeper with erosion control blankets that are heavy-duty, biodegradable, and net free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.

Contact Staff:

Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife
Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.



Christie L. Stanifer
Environ. Coordinator
Division of Fish and Wildlife

Date: February 26, 2019

THIS IS NOT A PERMIT

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR #: ER-21179-1

Request Received: November 21, 2019

Requestor: DLZ Indiana, LLC
Daniel J Stevens
2211 East Jefferson Boulevard
South Bend, IN 46615-2607

Project: Vernal Pike Connector Project: extension of the southern project limits about 550' south to North Gates Drive to include a portion of the locally funded Profile Parkway project, Bloomington; Des #1702957 & 1900406

County/Site info: Monroe

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment: Formal approval by the Department of Natural Resources under the regulatory programs administered by the Division of Water is not required for this project.

Natural Heritage Database: The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Fish & Wildlife Comments: All of the recommendations in our previous letter dated February 26, 2019, still apply.

Contact Staff: Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife
Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.



Christie L. Stanifer
Environ. Coordinator
Division of Fish and Wildlife

Date: December 20, 2019

Daniel Stevens

From: McWilliams, Robin <robin_mcwilliams@fws.gov>
Sent: Tuesday, February 26, 2019 10:10 AM
To: Daniel Stevens
Subject: Re: [EXTERNAL] Early Coordination Request - Vernal Pike Connector Project - Des. No.: 1702957 - Monroe County, Indiana

Dear Daniel,

This responds to your recent letter, requesting our comments on the aforementioned project.

These comments have been prepared under the authority of the Fish and Wildlife Coordination Act (16 U.S.C. 661 et. seq.) and are consistent with the intent of the National Environmental Policy Act of 1969, the Endangered Species Act of 1973, and the U. S. Fish and Wildlife Service's Mitigation Policy.

The project is within the range of the Indiana bat (*Myotis sodalis*) and northern long-eared bat (*Myotis septentrionalis*) and should follow the new Indiana bat/northern long-eared bat programmatic consultation process, if applicable (i.e. a federal transportation nexus is established). We will review that information once it is received. **Please note**, if tree clearing activities exceed 100 feet from edge of pavement or railroad ballast (and not more than 300 feet), mitigation will be necessary and the project will need to go through the formal process of the Indiana bat/northern long-eared bat rangewide programmatic consultation. I believe if you measure from the parking area (which would be appropriate in this situation), you will likely stay within 300 feet. Also, because the project is so near many Indiana bat and northern long-eared bat hibernacula, we recommend tree clearing be conducted Nov. 15-March 30 to avoid affecting bats during the fall swarming period.

To protect water quality we recommend using pollutant-trapping technology such as storm drain inserts, etc. to reduce runoff of urban pollutants directly to the stream system.

The project is in the karst area of Indiana and in or adjacent to an area that was noted during 1969 studies to have a sinking stream system. If any karst features are encountered, a karst survey should be conducted, with mitigation measures as necessary, in accordance with our 1993 Memorandum of Understanding. We also suggest that you coordinate with the Indiana Department of Environmental Management for groundwater and karst issues:

Scott Johanson, LPG # IN 1813
Geological Services
Science Services Branch
Office of Land Quality
Indiana Department of Environmental Management
(317) 234-0996 FAX: (317) 234-0428
(800) 451-6027
sjohanso@idem.IN.gov

Based on a review of the information you provided, the U.S. Fish and Wildlife Service has no objections to the project as currently proposed. However, should new information arise pertaining to project plans or a revised species list be published, it will be necessary for the Federal agency to reinitiate consultation. Standard recommendations are provided below.

We appreciate the opportunity to comment at this early stage of project planning. If project plans change such that fish and wildlife habitat may be affected, please recoordinate with our office as soon as possible. If you have any questions about our recommendations, please call (812) 334-4261 x. 207.

Sincerely,
Robin McWilliams Munson

Standard Recommendations:

1. Do not clear trees or understory vegetation outside the construction zone boundaries. **(This restriction is not related to the "tree clearing" restriction for potential Indiana Bat habitat.)**
2. Restrict below low-water work in streams to placement of culverts, piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap.

Culverts should span the active stream channel, should be either embedded or a 3-sided or open-arch culvert, and be installed where practicable on an essentially flat slope. When an open-bottomed culvert or arch is used in a stream, which has a good natural bottom substrate, such as gravel, cobbles and boulders, the existing substrate should be left undisturbed beneath the culvert to provide natural habitat for the aquatic community.
3. Restrict channel work and vegetation clearing to the minimum necessary for installation of the stream crossing structure.
4. Minimize the extent of hard armor (riprap) in bank stabilization by using bioengineering techniques whenever possible. If rip rap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat.
5. Implement temporary erosion and sediment control methods within areas of disturbed soil. All disturbed soil areas upon project completion will be vegetated following INDOT's standard specifications.
6. Avoid all work within the inundated part of the stream channel (in perennial streams and larger intermittent streams) during the fish spawning season (April 1 through June 30), except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season. No equipment shall be operated below Ordinary High Water Mark during this time unless the machinery is within the caissons or on the cofferdams.
7. Evaluate wildlife crossings under bridge/culverts projects in appropriate situations. Suitable crossings include flat areas below bridge abutments with suitable ground cover, high water shelves in culverts, amphibian tunnels and diversion fencing.

U.S. Fish and Wildlife Service
620 South Walker Street
Bloomington, Indiana 46403
812-334-4261 x. 207 Fax: 812-334-4273

Monday, Tuesday - 7:30a-3:00p
Wednesday, Thursday - telework 8:30a-3:00p

Daniel Stevens

From: McWilliams, Robin <robin_mcwilliams@fws.gov>
Sent: Thursday, December 5, 2019 12:25 PM
To: Daniel Stevens
Subject: Re: [EXTERNAL] Early Coordination Request - Vernal Pike Connector Project - Des. No.: 1702957 - Monroe County, Indiana

EXTERNAL: Message origin is from an external network. Use proper judgment and caution when opening attachments, clicking links, or responding to this email.

Dear Daniel,

Thank you for sending the updated project information for Vernal Pike Connector project (Des No. 1702957 and 1900406) which includes the extension of Sunrise Greeting Court south to Profile Parkway, connecting West Vernal Pike to Profile Parkway via a railroad overpass. Since tree clearing will occur beyond 300 feet from the edge of existing pavement, the Federal Highway Administration, Federal Railroad Administration, and Federal Transit Administration's rangewide programmatic consultation on transportation projects in the range of the Indiana bat and northern long-eared bat is not applicable.

These comments have been prepared under the authority of the Fish and Wildlife Coordination Act (16 U.S.C. 661 et. seq.) and are consistent with the intent of the National Environmental Policy Act of 1969, the Endangered Species Act of 1973, as amended, and the U. S. Fish and Wildlife Service's Mitigation Policy.

The project will include road and bridge construction on a new alignment over CSX Railroad (operated by Indiana Railroad). According to the new information, the southern project study limits have been extended approximately 550 feet south to North Gates Drive. This will incorporate a portion of the locally funded Profile Parkway Project into the impact assessment for the Vernal Pike Connector Project. The purpose of the project is to provide a connection between the existing commercial and industrial developments in the project vicinity.

The total project length of approximately 0.5 miles. The typical roadway section will consist of two 12-foot lanes with 8-inch concrete curb and gutter, a 5-foot sidewalk on the west side and a 10-foot Multi Use path on the east side of the proposed roadway, along with 3-foot bike lanes adjacent to the travel lanes. Retaining walls may be incorporated at the fill slopes of the railroad overpass bridge to optimize earth embankment cost and right-of-way. The project area north of the railroad tracks will be connected to existing storm sewers. The project area south of the railroad tracks will be connected to the new Profile Parkway storm sewer system.

Land use is industrial/manufacturing along Sunrise Greetings Court. Land from Sunrise Greetings Court south to the railroad tracks is undeveloped and partially wooded land. The land south of the tracks is former industrial land that is currently undeveloped and a combination of wooded land and old field. The anticipated project right-of-way needed for the Vernal Pike Connector is approximately 2.72 acres of permanent and 1.04 acres of temporary right-of-way. An additional approximately 1.82 acres of permanent right-of-way is required for the included segment of Profile Parkway.

GENERAL RECOMMENDATIONS

Based on a review of the information you provided, we recommend the following mitigation measures be included in the final project plans to minimize adverse impacts to fish and wildlife resources:

1. Avoid all work within the inundated part of the stream channel (in perennial streams and larger intermittent streams) during the fish spawning season (April 1 through June 30), except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season. No equipment should be operated below Ordinary High Water Mark during this time unless the machinery is within the caissons or on the cofferdams.

2. Restrict below low-water work to placement of piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap.

3. Restrict channel work and vegetation clearing to the minimum necessary.

4. Construct new structures with a widened span and benches on one or both sides to provide for wildlife crossing, if practical. The crossing should be above normal high water, relatively flat and with natural substrate suitable for use by a wide variety of wildlife.

5. If riprap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat.

6. Implement temporary erosion and siltation control devices such as placement of riprap check dams in drainage ways and ditches, installation of silt fences, covering exposed areas with erosion control materials, and grading slopes to retain runoff in basins.

7. Re-vegetate all disturbed soil areas immediately upon project completion, using native trees and shrubs in the riparian zone wherever feasible.

8. Post DO NOT DISTURB signs at the construction zone boundaries and do not clear trees or understory vegetation outside the boundaries.

To protect water quality we recommend using pollutant-trapping technology such as storm drain inserts, etc. to reduce runoff of urban pollutants directly to the stream system.

As previously mentioned, the project is in the karst area of Indiana and in or adjacent to an area that was noted during I69 studies to have a sinking stream system. If any karst features are encountered, a karst survey should be conducted, with mitigation measures as necessary, in accordance with our 1993 Memorandum of Understanding. We suggest that you continue to coordinate with the Indiana Department of Environmental Management for groundwater and karst issues.

THREATENED AND ENDANGERED SPECIES

The proposed project is within the range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (*Myotis septentrionalis*) (NLEB). There are numerous records of both species in Monroe County. In addition, the project is within a 10 mile buffer of designated Critical Habitat for the Indiana bat.

Indiana bats hibernate in caves then disperse to reproduce and forage in relatively undisturbed forested areas associated with water resources during spring and summer. Recent research has shown that they will inhabit fragmented landscapes with adequate forest for roosting and foraging. Young are raised in nursery colony roosts in trees, typically near drainage-ways in undeveloped areas. Like all other bat species in Indiana, the Indiana bat diet consists exclusively of insects. Critical habitat for the Indiana bat was designated on September 24, 1976; it consisted of 11 caves and two mines in six states (41 FR 41914, September 24, 1976).

The northern long-eared bat was listed as threatened under the Endangered Species Act (ESA) (87 Stat. 884, as amended; 16 U.S.C. 1531 *et seq.*). At this time, no critical habitat has been proposed for the NLEB. NLEBs typically roost singly or in colonies in cavities, underneath bark, crevices, or hollows of both live and dead trees and/or snags (typically ≥ 3 inches dbh). Males and non-reproductive females may also roost in cooler places, like caves and mines. This bat seems opportunistic in selecting roosts, using tree species based on presence of cavities or crevices or presence of peeling bark. It has also been occasionally found roosting in structures like barns and sheds (particularly when suitable tree roosts are unavailable). They forage for insects in upland and lowland woodlots and tree lined corridors. During the winter, NLEBs predominantly hibernate in caves and abandoned mine portals. Additional habitat types may be identified as new information is obtained.

There is suitable summer habitat for **both** of these species present throughout the area surrounding the project site, including wooded areas within the project boundary. The project is also near several Indiana bat and northern long-eared bat hibernacula, as well as within designated Indiana bat critical habitat. The project will not eliminate enough habitat to affect these species, but to avoid incidental take from removal of an occupied roost tree, tree clearing should be conducted between **Nov. 15-March 30**. If this measure is implemented we concur that the proposed project is not likely to adversely affect the Indiana bat or the northern long-eared bat.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act of 1973, as amended. If project plans change such that fish and wildlife habitat may be affected, please re-coordinate with our office as soon as possible. If you have any questions about our recommendations, please call Robin McWilliams Munson at (812) 334-4261 (Ext. 207).

Sincerely,

Robin McWilliams Munson

U.S. Fish and Wildlife Service
620 South Walker Street
Bloomington, Indiana 46403
812-334-4261 x. 207 Fax: 812-334-4273

Monday, Tuesday - 7:30a-3:00p

Wednesday, Thursday - telework 8:30a-3:00p



United States Department of the Interior

FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>



In Reply Refer To:

February 12, 2020

Consultation Code: 03E12000-2020-SLI-0798

Event Code: 03E12000-2020-E-03627

Project Name: Vernal Pike Connector

Subject: List of threatened and endangered species that may occur in your proposed project location, and/or may be affected by your proposed project

To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Section 7 of the Endangered Species Act of 1973 requires that actions authorized, funded, or carried out by Federal agencies not jeopardize federally threatened or endangered species or adversely modify designated critical habitat. To fulfill this mandate, Federal agencies (or their designated non-federal representative) must consult with the Service if they determine their project “may affect” listed species or critical habitat.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website <http://ecos.fws.gov/ipac/> at regular intervals during project planning and implementation and completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you

determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process.

For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*) and Migratory Bird Treaty Act (16 U.S.C. 703 *et seq.*), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at <http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html> to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List

Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

(812) 334-4261

Project Summary

Consultation Code: 03E12000-2020-SLI-0798

Event Code: 03E12000-2020-E-03627

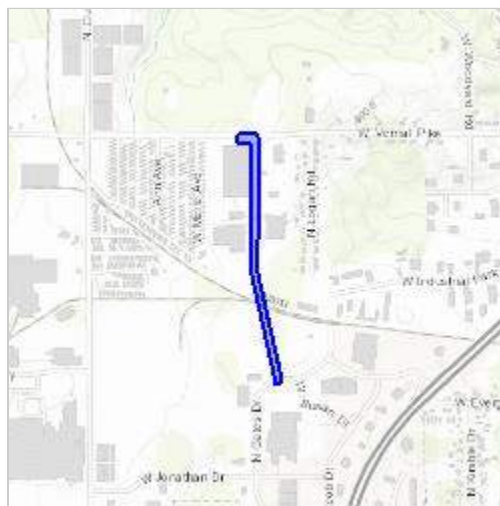
Project Name: Vernal Pike Connector

Project Type: TRANSPORTATION

Project Description: The project is the Vernal Pike Connector (Des. No. 1702957 & 1900406). The project does not qualify for IPaC use and requires standard coordination with USFWS since the limits of tree clearing extend beyond 300 feet from an existing roadway. The project information provided herein is abbreviated and included to generate an official species list. The project includes the extension of Sunrise Greeting Court south to Profile Parkway (which is currently under design), connecting West Vernal Pike to Profile Parkway and North Gates Drive via a railroad overpass. The project will include road and bridge construction on a new alignment over CSX Railroad, with a total project length of approximately 0.5 miles. This project is located in Monroe County, Indiana in Sections 25 and 36, Township 9 North, Range 2 West.

Project Location:

Approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/place/39.17538967707428N86.5764790128041W>



Counties: Monroe, IN

Endangered Species Act Species

There is a total of 2 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

Mammals

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. Your location overlaps the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5949 Species survey guidelines: https://ecos.fws.gov/ipac/guideline/survey/population/1/office/31440.pdf	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none"> ▪ Incidental take of the NLEB is not prohibited here. Federal agencies may consult using the 4(d) rule streamlined process. Transportation projects may consult using the programmatic process. See www.fws.gov/midwest/endangered/mammals/nleb/index.html Species profile: https://ecos.fws.gov/ecp/species/9045	Threatened

Critical habitats

There is 1 critical habitat wholly or partially within your project area under this office's jurisdiction.

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i>	Final

NAME	STATUS
https://ecos.fws.gov/ecp/species/5949#crithab	

Daniel Stevens

From: Haseeb A. Ghumman, PE, PTOE
Sent: Tuesday, July 7, 2020 12:42 PM
To: Daniel Stevens
Subject: FW: Monroe County Zoning Ordinance.pdf

From: Lisa Ridge <ljridge@co.monroe.in.us>
Sent: Tuesday, January 15, 2019 3:17 PM
To: Haseeb A. Ghumman, PE, PTOE <hghumman@dlz.com>; Paul Satterly <psatterly@co.monroe.in.us>
Cc: Drew S. Barth <dsbarth@dlz.com>; 1863.2010.90 <1863.2010.90@dlz.com>
Subject: RE: Monroe County Zoning Ordinance.pdf

I just spoke with Larry Wilson, Planning Director. Our road projects are exempt from the section of the ordinance. We would be fine to be able to cap it.

*LISA RIDGE, PUBLIC WORKS DIRECTOR
MONROE COUNTY PUBLIC WORKS DEPARTMENT
501 N. MORTON STREET, SUITE 216
BLOOMINGTON, INDIANA 47404
LJRIDGE@CO.MONROE.IN.US
(812)349-2555 FAX (812)349-2959
CELL (812)325-8132*



From: Haseeb A. Ghumman, PE, PTOE [<mailto:hghumman@dlz.com>]
Sent: Tuesday, January 15, 2019 11:23 AM
To: Lisa Ridge <ljridge@co.monroe.in.us>; Paul Satterly <psatterly@co.monroe.in.us>
Cc: Drew S. Barth <dsbarth@dlz.com>; 1863.2010.90 <1863.2010.90@dlz.com>
Subject: Monroe County Zoning Ordinance.pdf

Lisa/Paul,

Before we approach INDOT, wanted to run the attached by you. Please see chap 829 of attached. Do we need to follow these requirements or are these waived, if we were to cap the sinkhole as we discussed yesterday? Please advise, if we can't deviate from attached ordinance then we will need to shift the alignment west to avoid impacting the sinkhole.

Thanks,

Haseeb A. Ghumman, PE, PTOE | Transportation Department Manager

317-633-4120 x230 (office) | 317-633-4177 (fax) | 317-366-9166 (cell)

hghumman@dlz.com | www.dlz.com



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APPENDIX D

Section 106 Documentation



Vernal Pike Connector
From West Vernal Pike to Profile Parkway
Des. No.: 1702957 & 1900406
Monroe County, Indiana

Appendix D

**FEDERAL HIGHWAY ADMINISTRATION'S
SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties) AND
SECTION 106 FINDINGS AND DETERMINATIONS
AREA OF POTENTIAL EFFECT
ELIGIBILITY DETERMINATIONS
EFFECT FINDING
VERNAL PIKE CONNECTOR PROJECT
RICHLAND TOWNSHIP, MONROE COUNTY, INDIANA
DES. NO.: 1702957 and 1900406**

AREA OF POTENTIAL EFFECTS (Pursuant to 36 CFR Section 800.4(a)(1))

The Area of Potential Effects (APE) delineated for this project extends approximately 500 feet to the west and 1,000 feet to the east from the project end points along West Vernal Pike, then approximately 700 feet on each side of Sunrise Greeting Court until the beginning of new alignment, at which point the APE expands to roughly a 1350 feet radius around the location of the proposed elevated bridge crossing the railway, and continuing south to a point approximately 350 feet south of Jonathan Drive acting as the APE's southern boundary. Refer to the Section 800.11(d) Document, Figure 4 for a graphic depicting the APE.

ELIGIBILITY DETERMINATIONS (Pursuant to 36 CFR 800.4(c)(2))

INDOT, acting on behalf of FHWA, has determined that no historic properties are located within the APE.

EFFECT FINDING

INDOT, acting on FHWA's behalf has determined a "No Historic Properties Affected" finding is appropriate for this undertaking. INDOT respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of effect.

SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties)

This undertaking will not convert property from any Section 4(f) historic property to a transportation use; the INDOT, acting on FHWA's behalf, has determined the appropriate Section 106 finding is "No Historic Properties Affected"; therefore no Section 4(f) evaluation is required.

Anuradha Kumar V.

Anuradha V. Kumar, for FHWA
Manager
INDOT Cultural Resources

03/12/2020

Approved Date

**FEDERAL HIGHWAY ADMINISTRATION
DOCUMENTATION OF SECTION 106 FINDING OF
NO HISTORIC PROPERTIES AFFECTED
SUBMITTED TO THE STATE HISTORIC PRESERVATION OFFICER
PURSUANT TO 36 CFR Section 800.4(d)(1)
VERNAL PIKE CONNECTOR PROJECT
RICHLAND TOWNSHIP, MONROE COUNTY, INDIANA
DES. NO.: 1702957 and 1900406**

1. DESCRIPTION OF THE UNDERTAKING

This project is located in Monroe County, Indiana, within Richland Township. It can be found on the *Bloomington, Indiana* Quadrangle, in Sections 25 and 36, Township 9 North, Range 2 West. Refer to Exhibits 1 – 3. Refer to Exhibit 5 for project area photographs and key maps.

The Monroe County Board of Commissioners is proposing to utilize federal highway funding to construct the Vernal Pike Connector Project. This project includes the extension of Sunrise Greeting Court south to Profile Parkway, connecting West Vernal Pike to Profile Parkway via a railroad overpass (Des. No. 1702957 and 1900406). Based on comments received on the May 6, 2019 early coordination, the southern project study limits have been extended approximately 550 feet south to North Gates Drive. This will incorporate a portion of the locally funded Profile Parkway project into the impact assessment for the Vernal Pike Connector Project. This portion of the Profile Parkway project is being included to provide the Vernal Pike Connector project with independent utility in case the locally funded Profile Parkway project becomes delayed or never constructed. The entire Profile Parkway project is not included since the Vernal Pike Connector does not rely on the completion of Profile Parkway to provide independent utility.

The revised project limits begin approximately 500 feet west and 100 feet east of the intersection of West Vernal Pike and Sunrise Greetings Court, then proceed south along Sunrise Greetings Court for approximately 830 feet until reaching the cul-de-sac, then extend approximately 1,500 feet to the southeast on new alignment and cross over the CSX Railroad and continue to Profile Parkway. The project then extends approximately 550 feet south along Profile Parkway to North Gates Drive. Profile Parkway is currently under development with construction being completed prior to this project.

The project is necessary to provide a connection between the existing commercial and industrial developments in the project vicinity. As a result of the I-69 project, access to the area from SR-37-I-69 was cut off at West Vernal Pike and at West Whitehall Crossing Boulevard. Consequently, access to the existing commercial development, west of I-69, is now via Gates Drive only. One of the main connectivity issues is for traffic coming from the north and trying to access the commercial development immediately to the west of I-69 and north of 3rd Street, while also avoiding the at-grade rail crossings on Curry Pike. Additionally, traffic coming from the northwest on 17th Street/Vernal Pike would have to either deal with the rail crossings on Curry Pike or proceed down to 3rd Street east of I-69, which is difficult because there is no direct north-south road between 17th and 3rd. Street. In order to improve access and connectivity, Monroe County is currently in the process of designing the Profile Parkway extension (east of Curry Pike to Gates Drive). This is a locally funded project that will serve the future development along Profile Parkway and is expected to be completed prior to the construction of the proposed Vernal Pike Connector project. The Vernal Pike Connector project, which includes the extension of Sunrise Greeting Court and railroad overpass, will connect to the Profile Parkway extension. The portion of the Profile Parkway project that will connect the Vernal Pike Connector to North Gates Drive is being included in the impact assessment for the Vernal Pike Connector Project.

The proposed project includes the extension of Sunrise Greeting Court south to Profile Parkway (which is currently under design), connecting West Vernal Pike to Profile Parkway and North Gates Drive via a

railroad overpass. The project will include road and bridge construction on a new alignment over CSX Railroad (operated by Indiana Railroad), with a total project length of approximately 0.5 miles. The typical roadway section will consist of two 12-foot lanes with 8-inch concrete curb and gutter, a 5-foot sidewalk on the west side and a 10-foot multiuse path on the east side of the proposed roadway, along with 3-foot bike lanes adjacent to the travel lanes. Retaining walls may be incorporated at the fill slopes of the railroad overpass bridge to optimize earth embankment cost and right-of-way. The proposed road construction will tie into the existing roadway section of Sunrise Greeting Court at the existing cul-de-sac, approximately 830 feet south of West Vernal Pike. Construction of the 10-foot multiuse path will continue along the east side of Sunrise Greeting Court and will extend to West Vernal Pike. An eastbound right turn lane from West Vernal Pike to Sunrise Greeting Court may also be constructed along with a new traffic signal at the intersection of West Vernal Pike and Sunrise Greeting Court. The anticipated project right-of-way needed for the Vernal Pike Connector is approximately 2.72 acres of permanent and 1.04 acres of temporary right-of-way. An additional approximately 1.82 acres of permanent right-of-way is required for the included segment of Profile Parkway. The project area north of the railroad tracks will be connected to existing storm sewers. The project area south of the railroad tracks will be connected to the new Profile Parkway storm sewer system.

B. Applicability

Per FHWA-IN Procedures, Federal-aid highway construction projects qualify as “undertakings” as defined in 36 CFR 800.16(y) and are subject to review under FHWA-IN/INDOT Section 106 Procedures (herein referred to as the Procedures). Federal-aid funds would be used for planning and/or construction of the proposed improvements. Section 106 is applicable.

C. Area of Potential Effects (APE)

Per 36 CFR Section 800.4(a)(1), the APE delineated for this project extends approximately 500 feet to the west and 1,000 feet to the east from the project end points along West Vernal Pike, then approximately 700 feet on each side of Sunrise Greeting Court until the beginning of new alignment, at which point the APE expands to roughly a 1350 feet radius around the location of the proposed elevated bridge crossing the railway, and continuing south to a point approximately 350 feet south of Jonathan Drive acting as the APE’s southern boundary (Exhibit 4).

2. EFFORTS TO IDENTIFY HISTORIC PROPERTIES

A. Historic Properties Report

H&H Associates, LLC (H&H) completed a short-form Historic Properties Report (HPR) that provided recommendations concerning the historic significance of the properties within the APE. H&H initiated identification efforts in October 2018 by reviewing the National Register of Historic Places (NRHP), the Indiana Register of Historic Sites and Structures, the State Historic Architectural and Archaeological Research Database (SHAARD), the Indiana Historical Bureau’s Historical Markers Database, and the 2015 Monroe County Interim Historic Sites and Structures Inventory for previously-identified properties. Primary and secondary documentary research included numerous published county and local histories, historical and current atlases and maps, and online resources. Additionally, on October 16, 2018 the historian conducted a field survey by walking all the streets within the APE and taking photographs to identify and evaluate any historic resources present. As a result of identification and evaluation efforts, no properties within the project APE were recommended eligible for listing on the NRHP. The Abstract and Conclusions section of the HPR are presented as Exhibit 6, Pages 1 - 3.

An Addendum HPR (H&H, 2019) was prepared for the expansion of the APE to include a portion of the locally funded Profile Parkway Project. As a result of the historic property identification and evaluation efforts, no above-ground resources are recommended as eligible for listing in the NRHP in the expanded project area. The Abstract and Conclusions section of the Addendum HPR are presented as Exhibit 6, Pages 4 - 6.

B. Archaeological Survey

NS Services, LLC conducted an archaeological records check and Phase Ia Archaeological Field Reconnaissance of the project area. The report notes that the archaeological reconnaissance located no archaeological sites within the project area and recommended that the project be allowed to proceed as planned. The Conclusions section from the Phase Ia Archaeological Field Reconnaissance of the project area is attached as Exhibit 7, Pages 1 - 2.

An Addendum Phase Ia Field Reconnaissance (NS Services, LLC, 2019) was prepared for the expansion of the study limits to include a portion of the locally funded Profile Parkway Project. As a result of these efforts, no sites were recommended as eligible for listing in the NRHP in the expanded project area and no further work is recommended. The Conclusions section from the Addendum Phase Ia Archaeological Field Reconnaissance of the project area is attached as Exhibit 7, Pages 3 – 4.

C. Consultation

Per Section 106 and 36 CFR 800, in addition to the State Historic Preservation Officer (SHPO), the entities listed below were invited to participate as consulting parties for this undertaking. Invited entities were requested to indicate whether the entities agreed or did not agree to participate as a consulting party, within 30 days of receipt of the invitation. It was noted that if the desire to participate as a consulting party was not indicated, the entities would not be considered a consulting party and would not receive further information about the undertaking unless the scope changed.

Invited Section 106 Consulting Parties	Status
Monroe County Board of Commissioners	No Response - Declined
Monroe County Highway Superintendent	No Response - Declined
Monroe County Historian	No Response - Declined
Monroe County History Center	No Response - Declined
Bloomington/Monroe County MPO	Accepted
Indiana Landmarks Central Regional Office	Accepted
Bloomington Restorations, Inc.	No Response - Declined
Downtown Bloomington Commission	No Response - Declined
Monroe County Historic Preservation Board of Review	Accepted
Preservation Development, Inc.	No Response - Declined
City of Bloomington, Historic Preservation Program Manager, Department of Housing and Neighborhood Development	No Response - Declined
City of Bloomington Street Department	No Response - Declined
Delaware Tribe of Indians, Oklahoma	No Response - Declined
Eastern Shawnee Tribe of Oklahoma	No Response - Declined
Miami Tribe of Oklahoma	Accepted
Peoria Tribe of Indians of Oklahoma	No Response - Declined
Pokagon Band of Potawatomi Indians	No Response - Declined

Initial Consultation

A hard copy of the early coordination letter was sent to the SHPO on May 6, 2019, and the other consulting parties received it via email on May 9, 2019 (Exhibit 8, Pages 1 - 11). The consulting parties could view the HPR by accessing INDOT's Section 106 document posting website IN SCOPE at <http://erms.indot.in.gov/Section106Documents/>. The early coordination letter noted the availability of the HPR, which contains information regarding the project's APE, and the identification of historic properties within the APE. Recipients of this consultation request were requested to provide comments within 30 days.

Per Section 106 and 36 CFR 800, the SHPO is an automatic consulting party. The SHPO was requested to identify the need to include additional consulting parties. No other consulting parties were identified.

In a letter dated May 9, 2019 (Exhibit 8, Page 12), the Bloomington/Monroe County MPO indicated that the proposed project is wholly consistent with the Bloomington-Monroe County 2040 Metropolitan Transportation Plan, the FY 2018-2021 Transportation Improvement Program, and the Draft FY 2020-2024 Improvement Program. No concerns regarding historic resources were raised by the MPO.

In a letter dated May 29, 2019 (Exhibit 8, Page 13), the Monroe County Historic Board of Review concurred with the report's recommendations, and it agrees to a finding of no adverse effect to historic properties.

In a letter dated June 6, 2019 (Exhibit 8, Page 14), the Indiana Landmarks Central Regional Office concurred that there are no properties listed in or eligible for listing in the NRHP within the proposed APE for the project. Indiana Landmarks also recommended considering enlarging the APE to including Profile Parkway, a locally funded project. In response to this comment, the APE has been enlarged to include the segment of Profile Parkway required to connect the Vernal Pike Connector to North Gates Drive. This was done to provide the Vernal Pike Connector project with independent utility in case the locally funded Profile Parkway project becomes delayed or never constructed. The entire Profile Project is not included since the Vernal Pike Connector does not rely on the completion of Profile Parkway to provide independent utility.

The SHPO provided comments dated July 12, 2019 (Exhibit 8, Pages 15 - 16) indicating that the APE in the HPR appears to be of appropriate size to encompass the geographic area in which direct and indirect effects of this project could occur. The SHPO also agreed with the HPR's recommendations that there do not appear to be any individual, above-ground properties or historic districts within the APE that would be eligible for inclusion in the NRHP. The SHPO also concurred with the recommendations of the archeological report and agreed that no further archaeological investigations appear necessary at the proposed project area.

No responses were received from the other consulting parties regarding the initial consultation.

Additional Consultation

Additional consultation was performed due to changes in the project limits. Based on comments received on the May 6, 2019 early coordination, the southern project study limits have been extended approximately 550 feet south to North Gates Drive. This will incorporate a portion of the locally funded Profile Parkway project into the impact assessment for the Vernal Pike Connector Project. This portion of the Profile Parkway project is being included to provide the Vernal Pike Connector project with independent utility in case the locally funded Profile Parkway project becomes delayed or never constructed. All consulting parties were sent the additional consultation letter. A hard copy of the additional consultation letter was sent to the SHPO on January 2, 2019, and the other consulting parties received it via email on January 2, 2019 (Exhibit 9, Pages 1 - 11). The consulting parties could view the Addendum HPR by accessing INDOT's Section 106 document posting website IN SCOPE at <http://erms.indot.in.gov/Section106Documents/>. The additional consultation letter noted the availability of the Addendum HPR, which contains information regarding the project's APE, and the identification of historic properties within the APE. Recipients of this consultation request were requested to provide comments within 30 days.

In a letter dated January 6, 2020 (Exhibit 9, Page 12), the Miami Tribe indicated they have no objection to the project and are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to the project site. However, as this project is within the aboriginal homelands of the Miami Tribe, if any human remains or Native American cultural items falling under the Native American Graves Protection and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of

this project, the Miami Tribe requests immediate consultation with the entity of jurisdiction for the location of discovery.

The SHPO provided comments dated February 6, 2020 (Exhibit 9, Pages 13 - 14) indicating that the revised APE in the HPR appears to be of appropriate size to encompass the geographic area in which direct and indirect effects of this project could occur. The SHPO also agreed with the HPR addendum's recommendations that there do not appear to be any individual, above-ground properties or historic districts within the APE that would be eligible for inclusion in the NRHP. The SHPO also concurred with the recommendations of the addendum archeological report and agreed that no further archaeological investigations appear necessary at the proposed project area.

No responses were received from the other consulting parties regarding the additional consultation.

A public notice will be issued in a locally available newspaper concurrently with the review of this document by consulting parties. The public notice will request comments from the general public and will provide the appropriate contact information to do so. This document will be revised as necessary after the public notice if comments are received.

Following the 30-day comment period(s), if there is no disagreement with the "No Historic Properties Affected" finding from consulting parties or the public, the Section 106 process will be considered complete.

3. BASIS FOR FINDING

Investigations conducted by qualified professionals did not identify the existence of historic properties within the project's APE, and no consulting party expressed objection to the recommendations of the HPR or the archaeological report. Therefore, a "No Historic Properties Affected" finding has been made for this undertaking.

A. Continued Consultation

INDOT's Findings, made on behalf of FHWA, and supporting Section 800.11(d) documentation are hereby provided to the SHPO and consulting parties for a final 30-day consultation/comment period.

LIST OF EXHIBITS

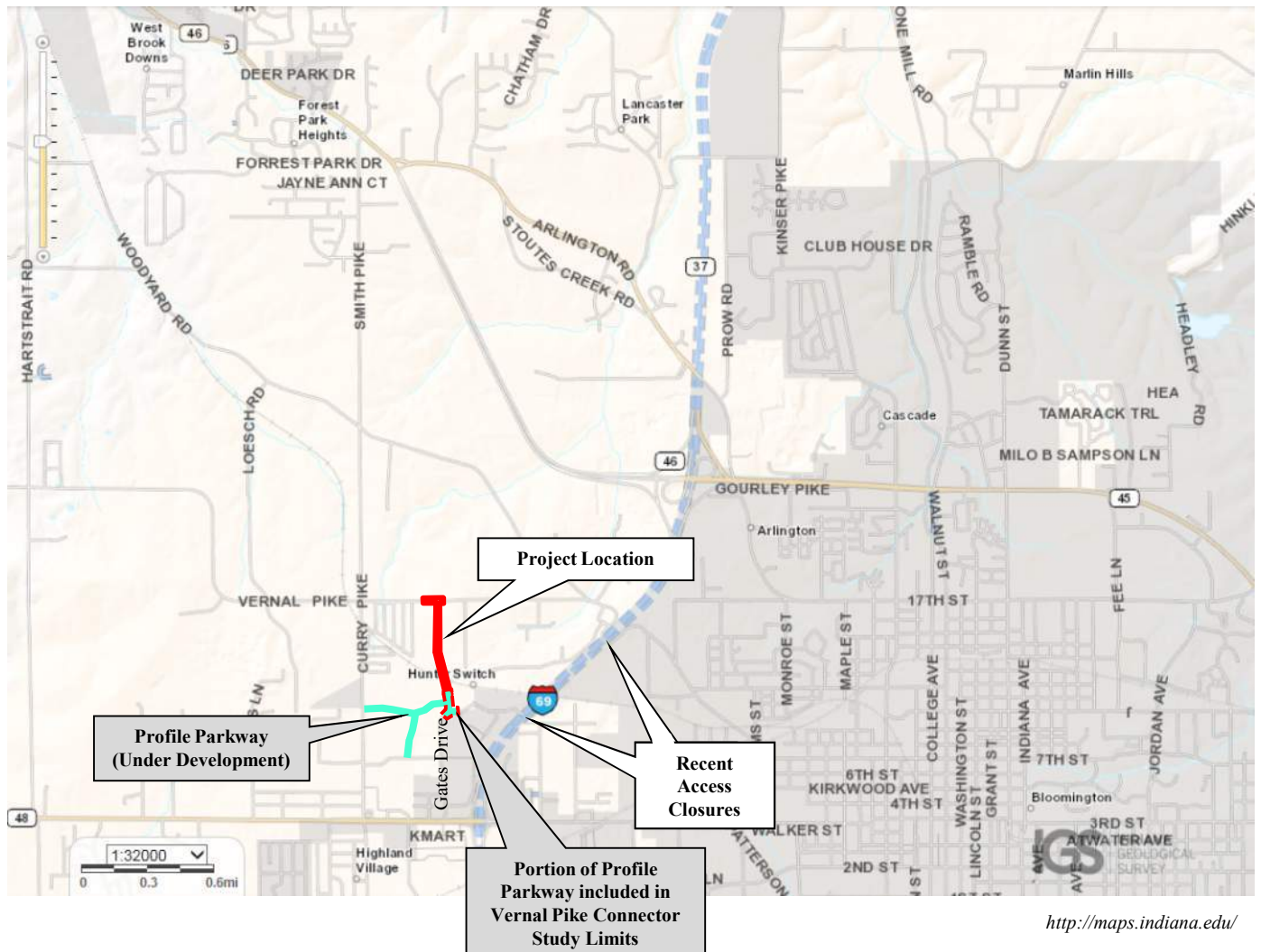
Exhibit 1: Location Map
Exhibit 2: USGS Quadrangle Map
Exhibit 3: Aerial Map Depicting Project Limits
Exhibit 4: Area of Potential Effects (APE)
Exhibit 5: Site Photographs and Key Map
Exhibit 6: Historic Properties Report Abstract and Conclusion
Exhibit 7: Archaeological Results and Recommendations
Exhibit 8: Consultation Letter and Correspondence
Exhibit 9: Additional Consultation Letter and Correspondence

Monroe County



Location Map

Project Location

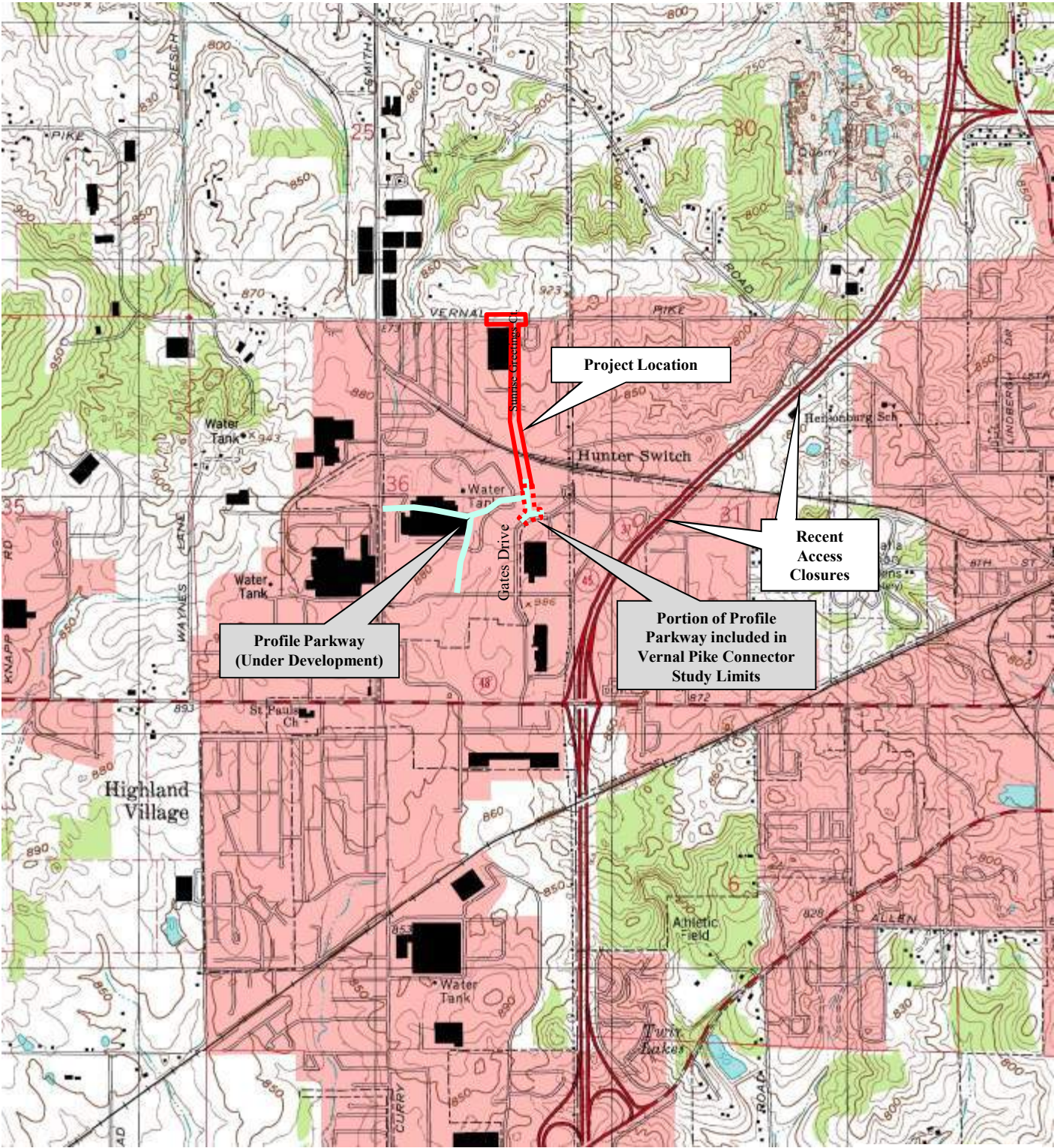


Vernal Pike Connector
From West Vernal Pike to Profile Parkway
Des. No.: 1702957 & 1900406
Monroe County, Indiana

Scale: NTS

Exhibit: 1

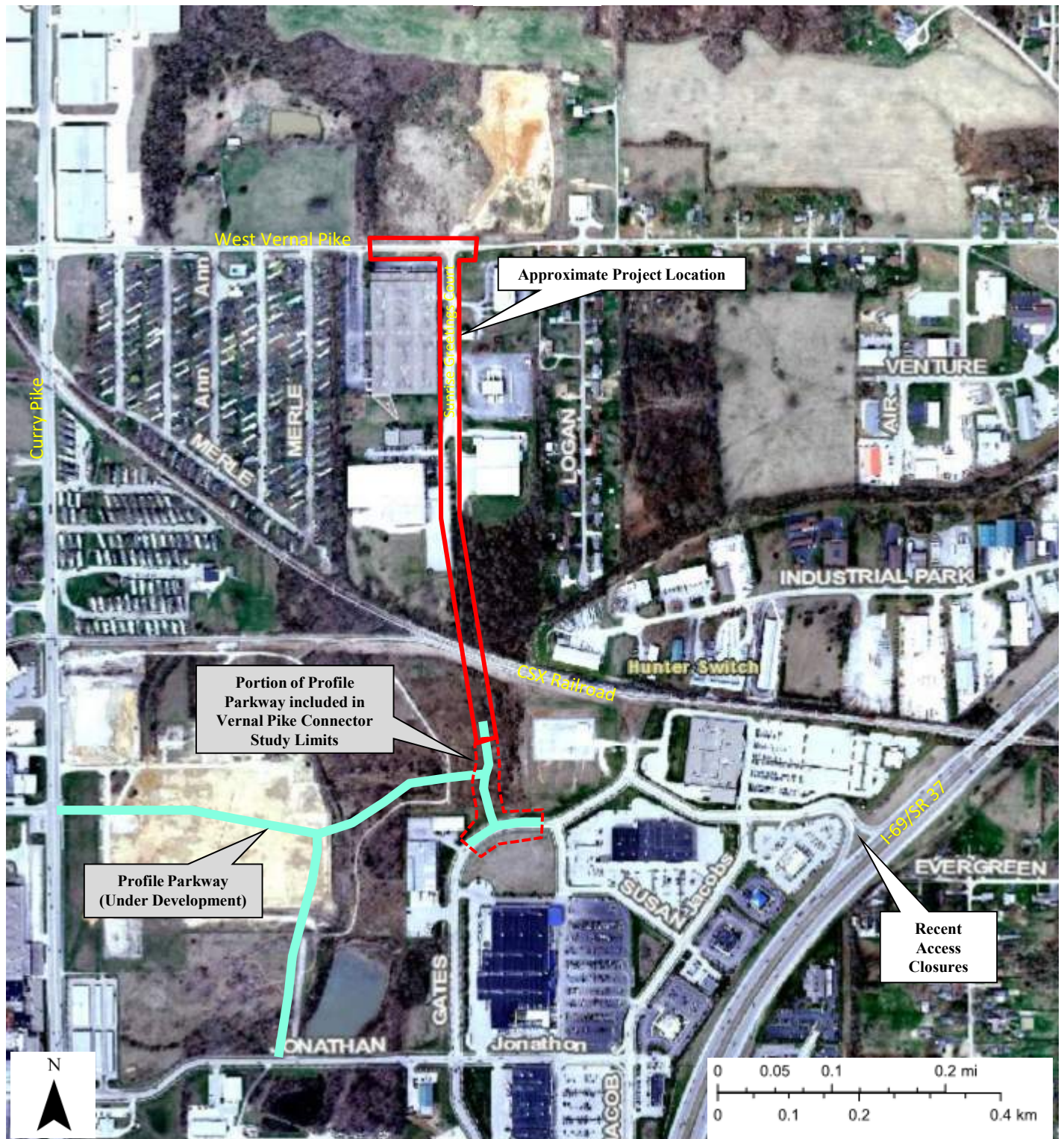
USGS Quadrangle Map



USGS Bloomington Quadrangle Map

	<p>Vernal Pike Connector From West Vernal Pike to Profile Parkway Des. No.: 1702957 & 1900406 Monroe County, Indiana</p>	<p>Scale: 1"=2000'</p>
		<p>Exhibit: 2</p>

Aerial



Vernal Pike Connector
From West Vernal Pike to Profile Parkway
Des. No.: 1702957 & 1900406
Monroe County, Indiana

Scale: See Map

Exhibit: 3

APE

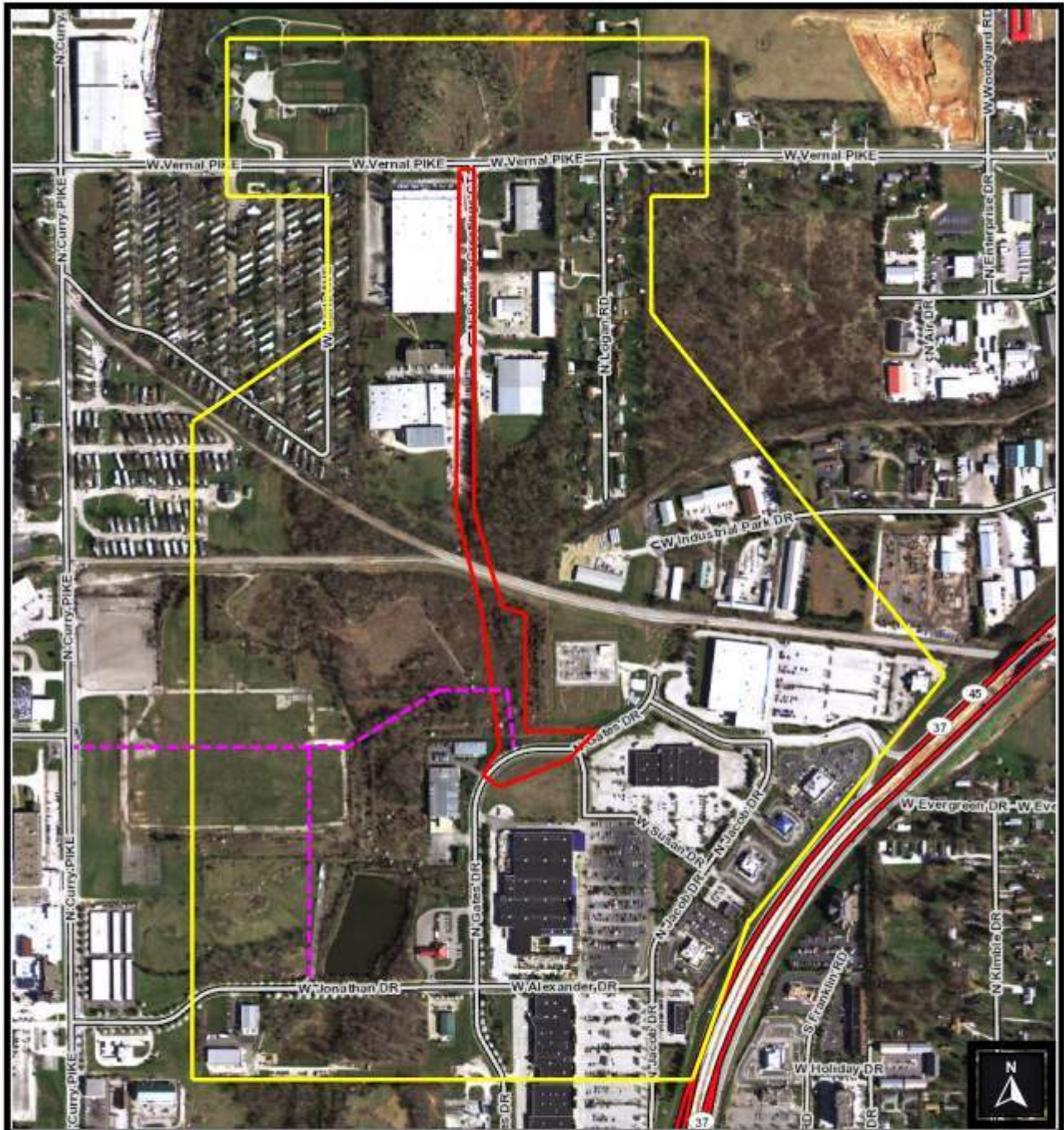


Fig B.2: Revised APE
Scale: 1 inch = 6,000 feet

2016 aerial image courtesy of Monroe County Elevate GIS: <https://monroein.elevatemaps.io/>

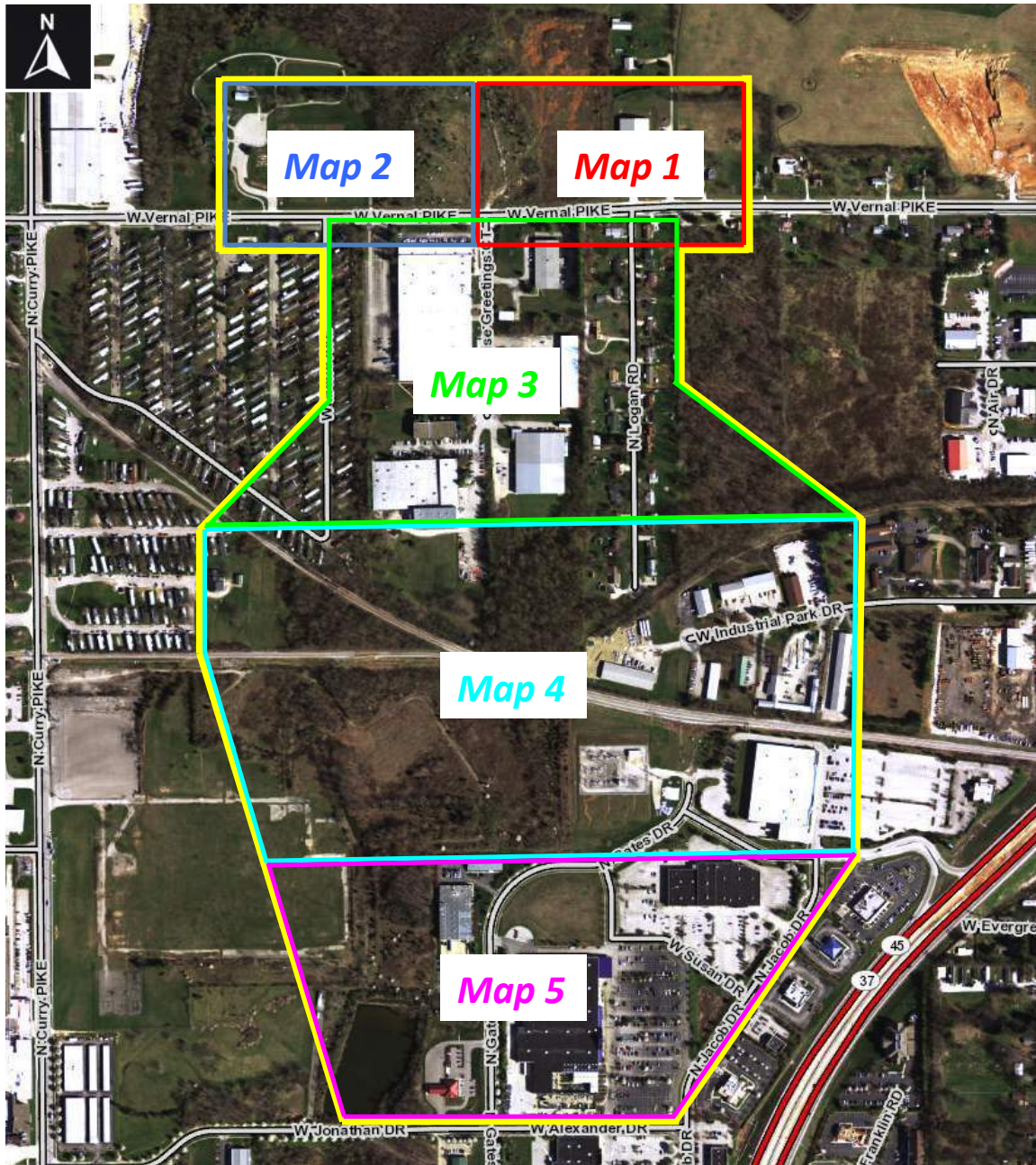


Vernal Pike Connector
 From West Vernal Pike to Profile Parkway
 Des. No.: 1702957 & 1900406
 Monroe County, Indiana

Scale: See Map

Exhibit: 4

Key Maps and Project Site Photographs

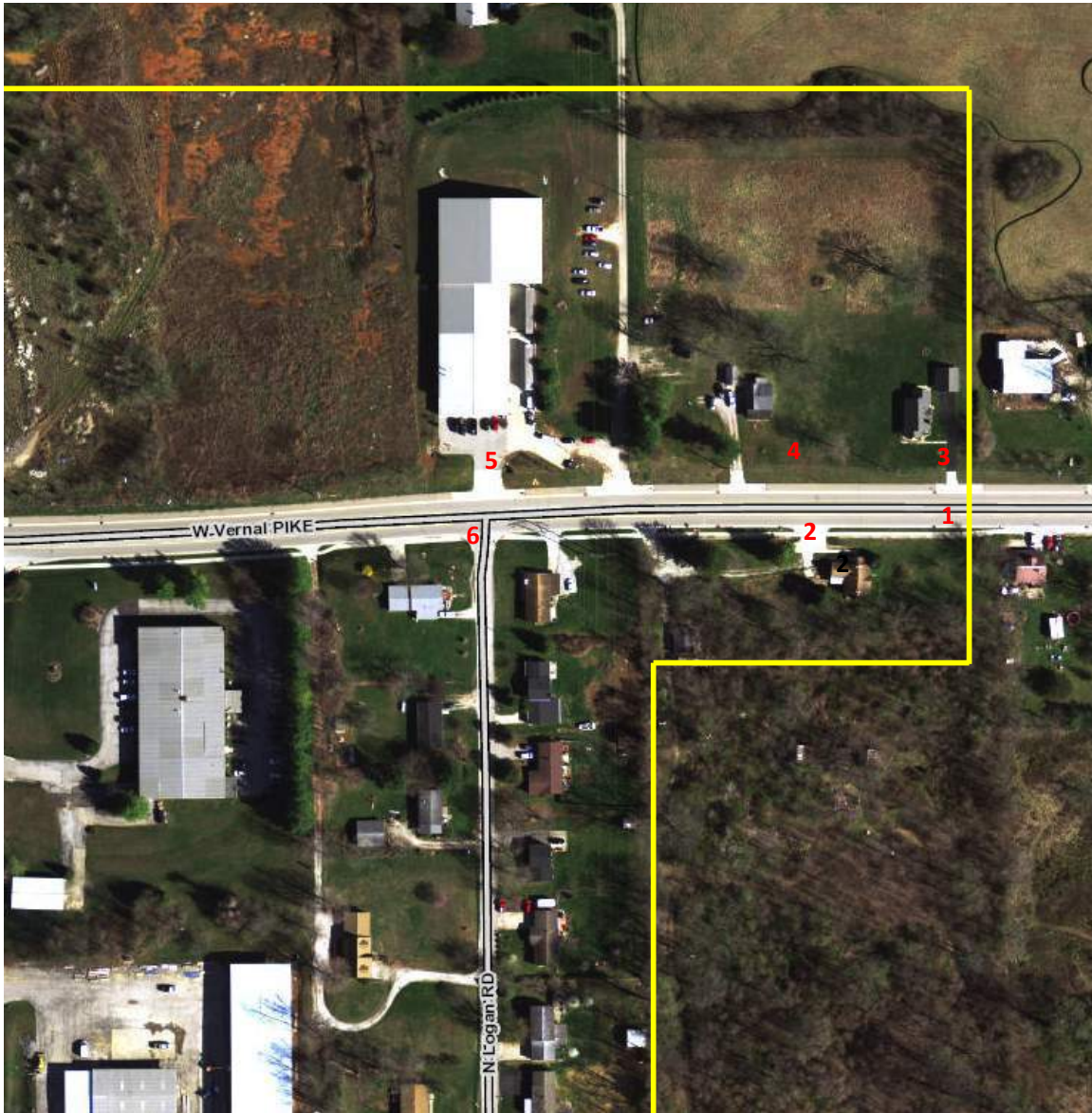


2016 Aerial map identifying each key map within the APE (in yellow)

Scale: 1 inch = 4,000 feet

All aerial images from Monroe County GIS Elevate:

<https://monroein.elevatemaps.io/#extent=3097359.7455385593,3095562.8705385593,1432690.736609941,1431117.8199432746,2245>

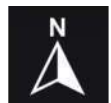


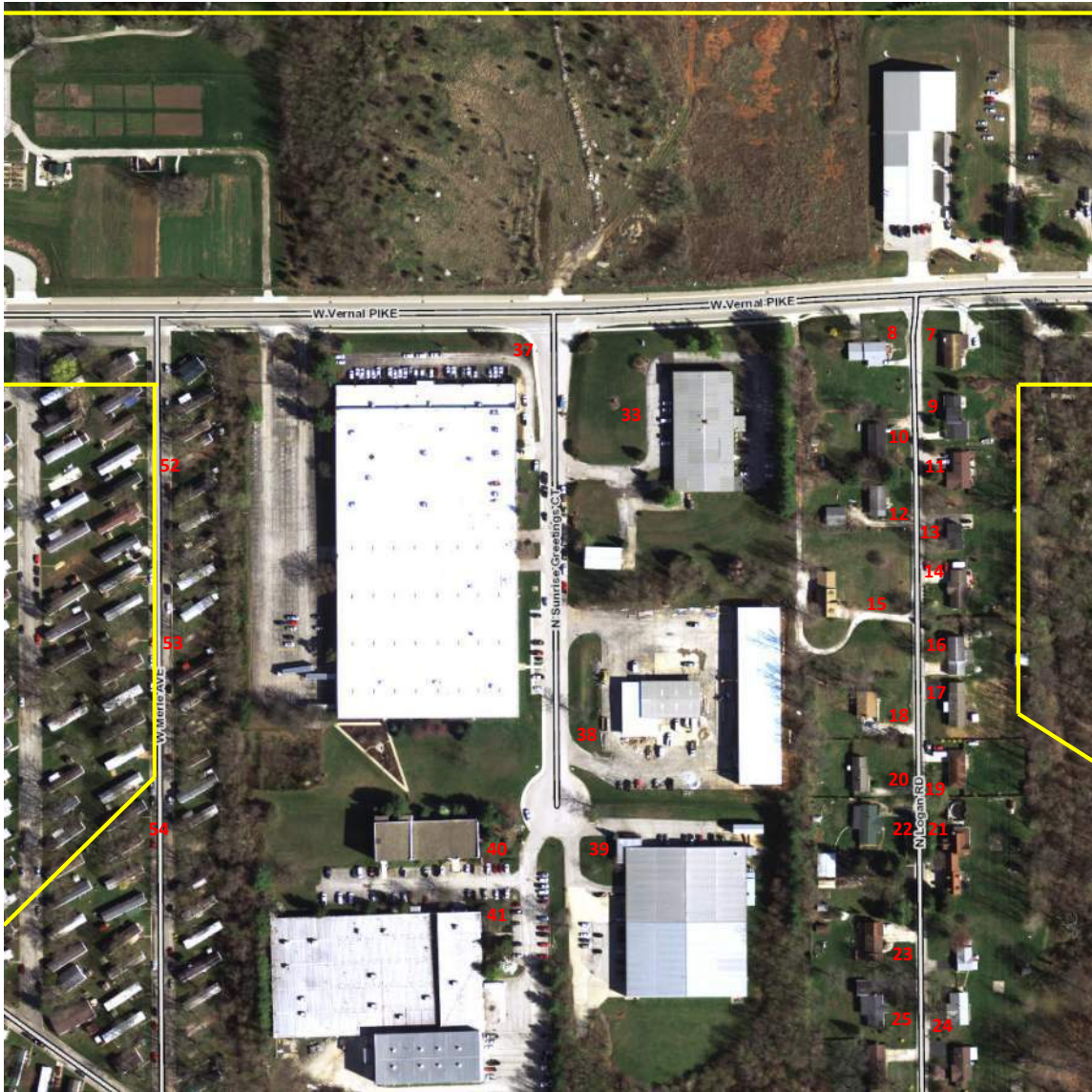
Map 1 showing location of photographs 1-6
Scale: 1 inch = 2,000 feet





Map 2 showing location of photographs 34-36, 43-51
Scale: 1 inch = 2,000 feet





Map 3 showing location of photographs 7-25, 33, 37-41, 52-54

Scale: 1 inch = 2,000 feet





Map 4 showing location of photographs 26-33, 42, 55-64, 70-81
Scale: 1 inch = 2,000 feet



Map 5 showing location of photographs 65-69
Scale: 1 inch = 2,000 feet



A.1: W Vernal Pike facing west from the east end of the APE



A.2: 3633 W Vernal Pike facing southeast



A.3: 3633 W Vernal Pike facing northwest



A.4: 3640 W Vernal Pike facing northwest



A.5: 3808 W Vernal Pike facing northwest



A.6: View of N Logan Rd facing south from W Vernal Pike



A.7: 1320 N Logan Rd facing southeast



A.8: 1315 N Logan Rd (105-055-19149, rated C) facing southwest



A.9: 1314 N Logan Rd facing southeast



A.10: 1309 N Logan Rd facing southwest



A.11: 1308 N Logan Rd (105-055-19150, rated C) facing southeast



A.12: 1303 N Logan Rd facing northwest



A.13: 1218 N Logan Rd facing southeast



A.14: 1212 N Logan Rd (105-055-19149, rated C) facing southeast



A.15: 1207 N Logan Rd facing northwest



A.16: 1206 N Logan Rd facing southeast



A.17: 1200 N Logan Rd facing southeast



A.18: 1201 N Logan Rd facing northwest



A.19: 1180 N Logan Rd facing northeast



A.20: 1165 N Logan Rd facing northwest



A.21: 1130 N Logan Rd facing southeast



A.22: 1135 N Logan Rd facing west



A.23: 1105 N Logan Rd facing northwest



A.24: 1060 N Logan Rd facing northeast



A.25: 1075 N Logan Rd facing northwest



A.26: 1052 N Logan Rd facing southeast



A.27: 1055 N Logan Rd facing west



A.28: 1000 N Logan Rd (105-055-19104, rated C) facing southeast



A.29: 1045 N Logan Rd facing northwest



A.30: 980 N Logan St facing southeast



A.31: 1035 N Logan St facing southwest



A.32: View of N Logan St facing north from the cul-de-sac



A.33: 3895 W Vernal Pike facing east



A.34: W Vernal Pike and Sunrise Greetings Ct intersection facing east



A.35: W Vernal Pike and Sunrise Greetings Ct intersection facing west



A.36: Sunrise Greetings Ct facing south from W Vernal Pike



A.37: 3963 W Vernal Pike facing southwest



A.38: 1200 N Sunrise Greetings Ct facing northeast



A.39: 1150 N Sunrise Greetings Ct facing southeast



A.40: 1145 N Sunrise Greetings Ct facing northwest



A.41: 1141 N Sunrise Greetings Ct facing southwest



A.42: View facing south from the end point of N Sunrise Greetings Ct



A.43: Will Detmer Park facing east



A.44: Will Detmer Park facing north



A.45: Will Detmer Park facing west



A.46: Entrance to Will Detmer Park from W Vernal Pike facing northeast



A.47: W Vernal Pike facing east from the west end of the APE



A.48: Entrance to Garden Hill Mobile Home Park facing southeast from W Vernal Pike



A.49: Entrance to Garden Hill Mobile Home Park facing southwest from W Vernal Pike



A.50: East side of W Merle Avenue facing southeast from W Vernal Pike



A.51: West side of W Merle Avenue facing south from W Vernal Pike



A.52: East side of W Merle Avenue facing northeast



A.53: East side of W Merle Ave facing southeast



A.54: W Merle Ave facing south



A.55: W Merle Ave facing north



A.56: W Merle Ave facing west



A.57: South side of W Merle Ave facing west



A.58: South side of W Merle Ave facing west



A.59: W Merle Ave facing southeast



A.60: View of unnamed road looking north from W Merle Ave



A.61: Mobile home park, N Curry Pike, facing northwest from railroad tracks



A.62: Mobile home park, N Curry Pike, facing southwest from railroad tracks



A.63: Mobile home park, N Curry Pike, facing west from end of the road



A.64: Ellettsville Fire Department, 900 N Curry Pike, facing southeast



A.65: Bank building, 301 N Gates Dr, facing west



A.66: Commercial building, 350 N Gates Dr, facing northwest



A.67: Commercial building, 3100 W Susan Dr, facing northeast



A.68: Manufacturing buildings, 401 N Gates Dr, facing northwest (1 of 2)



A.69: Manufacturing buildings, 401 N Gates Dr, facing west (2 of 2)



A.70: Utility substation, W Jonathan Dr, facing north



A.71: Commercial building, 3140 W Whitehall Crossing Blvd, facing west



A.72: W Industrial Park Dr facing southwest



A.73: Commercial building, 2121 W Industrial Park Dr, facing south (1 of 2)



A.74: Commercial building, 2121 W Industrial Park Dr, facing south (2 of 2)



A.75: Commercial building, 2140 W Industrial Park Dr, facing north



A.76: Commercial building, 2115 W Industrial Park Dr, facing south



A.77: Commercial building, 2120-2126 W Industrial Park Dr, facing northwest (1 of 2)



A.78: Commercial building, 2120-2126 W Industrial Park Dr, facing northeast (2 of 2)



A.79: Commercial building, 2118 W Industrial Park Dr, facing north



A.80: Commercial building, 2025 W Industrial Park Dr, facing southwest



A.81: W Industrial Park Dr facing northeast from the cul-de-sac

Photograph Key Map and Project Site Photographs



2016 aerial of the revised portion of the APE with photograph 1-13 locations' labeled
Scale: 1 inch = 3,000 feet

Image courtesy of Monroe County Elevate GIS website at:

<https://monroein.elevatemaps.io/#extent=3101148.95833333335,3092336.45833333335,1433409.8958333337,1427170.3125000002,2245>



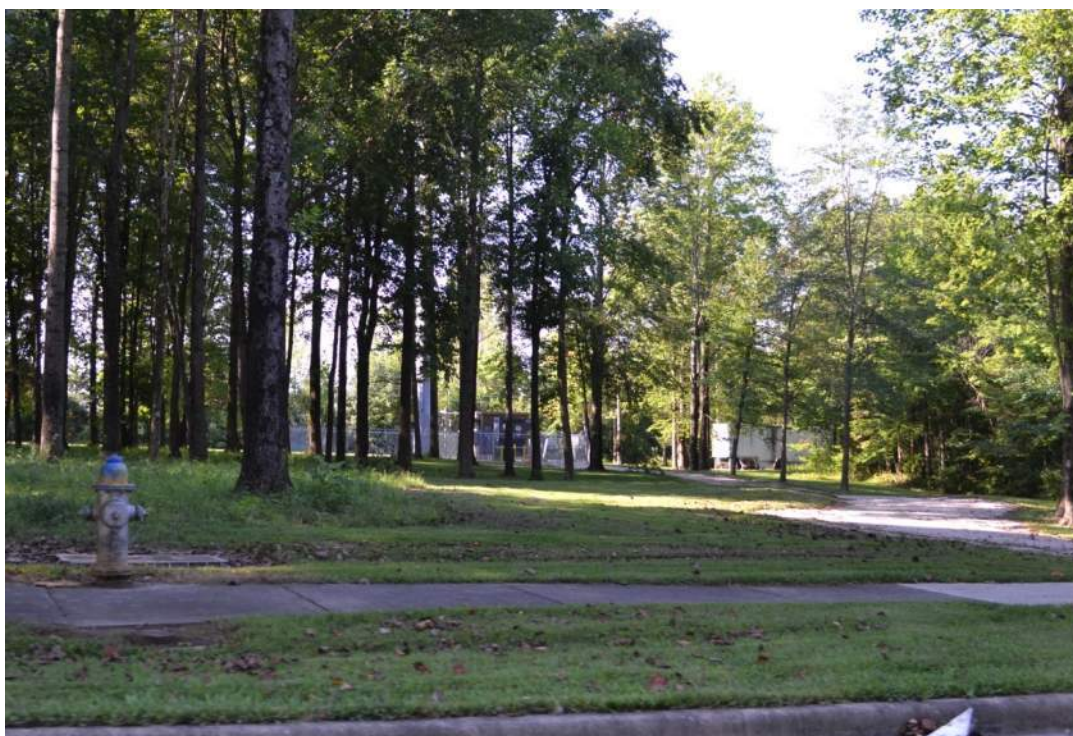
A.1: Future location of Profile Parkway, facing east from Curry Pike



A.2: Office building, 3704 W Jonathan Dr facing north, rated Non-Contributing (NC) *Note: this building was built after 2016, and thus it is not on the photograph key map*



A.3: Office building, 3701 W Jonathan Dr facing south, rated NC



A.4: Utility building, 3399 W Jonathan Dr facing south, rated NC



A.5: Commercial building, 3301 W Jonathan Dr facing SW, rated NC



A.6: Whitehall Crossing shopping center, 280-140 N Gates Dr facing SW, rated NC



A.7: Facing west from W Alexander Dr toward N Gates Dr



A.8: Shopping center, 260 N Jacob Dr facing SE, rated NC



A.9: Restaurant, 320 N Jacob Dr facing east, rated NC



A.10: Medical building, 330 N Jacob Dr facing east, rated NC



A.11: Restaurant, 360 N Jacob Dr facing east, rated NC



A.12: Bank (foreground), 370 N Jacob Dr, and restaurant (background), 380 N Jacob Dr, facing NE; both buildings are rated NC



A.13: Restaurant, 2980 W Whitehall Crossing Blvd, facing east, rated NC

**Pages Excerpted from the Historic Properties
Report (H&H Associates. LLC, 2018)**

And

**Pages Excerpted from the Addendum Historic Properties
Report (H&H Associates. LLC, 2019)**



Vernal Pike Connector
From West Vernal Pike to Profile Parkway
Des. No.: 1702957 & 1900406
Monroe County, Indiana

Exhibit 6

**HISTORIC PROPERTY SHORT REPORT
VERNAL PIKE CONNECTOR PROJECT
CITY OF BLOOMINGTON, MONROE AND RICHLAND
TOWNSHIPS, MONROE COUNTY, INDIANA**

DES NO: 1702957

02/17/2019

PREPARED FOR DLZ INDIANA, LLC

Candace Hudziak

H&H Associates, LLC
Principal Investigator and Author: Candace Hudziak, M.A.
331 N East Street
Greenfield, IN 46140
317.443.4123
hh.past12@gmail.com

Abstract

Vernal Pike Connector Project

City of Bloomington, Bloomington and Richland Townships, Monroe County, Indiana

This report documents the identification and evaluation efforts for properties included in the Area of Potential Effects (APE) for the Vernal Pike Connector Project. Above-ground resources located within the project APE were identified and evaluated in accordance with Section 106, National Historic Preservation Act (NHPA) of 1966, as amended, and the regulations implementing Section 106 (36 CFR Part 800).

As a result of the NHPA, as amended, and CFR Part 800, federal agencies are required to take into account the impact of federal undertakings upon historic properties in the area of the undertaking. Historic properties include buildings, structures, sites, objects, and/or districts that are eligible for or listed on the National Register of Historic Places (NRHP). As this project is receiving funding from the Federal Highway Administration (FHWA), it is subject to a Section 106 review.

The APE contains no properties listed on the NRHP. The APE contains no properties that are recommended eligible for listing on the NRHP.

or ethnicity. Logan Road does not appear to possess a direct or specific correlation to a theme that would set it apart and make it eligible.

Both mobile home parks have curvilinear streets and homogeneous designs, and the larger Garden Hill Mobile Home Park has mature shade trees that create a park-like setting. However, there is a distinct lack of continuity within each community since many of the mobile homes are larger, newer models with vinyl siding and windows. There is no evidence to show these mobile home parks were built for a specific need, such as worker housing for an area industry or short-term military or governmental housing. They are not especially close to the Indiana University campus to make them suitable for off-campus student housing when built in the late 1960s. Nor were either constructed near a major thoroughfare for commuting, as State Road 37 was not upgraded into a four-lane highway until the mid-1970s.⁴ Their strongest historical connection is likely with issues of class and socio-economic divisions that occurred, largely by design, in most postwar suburban developments, as well as with Indiana's robust history of postwar manufactured housing communities. Perhaps more research would provide a direct or specific connection with these larger themes, but unfortunately that is beyond the scope of this document. Thus at this time the historian does not recommend either mobile home park as eligible for listing as historic districts under the "Residential Planning and Development in Indiana, 1940-1973" Multiple Property Listing due to a lack of significance and integrity.

Conclusion

The APE contains no properties listed in the National Register.

As a result of identification and evaluation efforts for this project, no properties are recommended eligible for listing in the National Register.

⁴ Gene K. Hallock, "Indiana State Highway Commission Programs—Past and Future," n.d., available under <https://docs.lib.purdue.edu/cgi/viewcontent.cgi?article=3453&context=roadschool> [accessed November 2018].

Historic Property Short Report Addendum
Vernal Pike Connector Project
City of Bloomington, Monroe and Richland Townships, Monroe
County, Indiana

Des No. 1702957

H&H Associates, LLC
331 N East Street
Greenfield, IN 46140
hh.past12@gmail.com
Candace Hudziak, M.A.
Principal Investigator

Prepared for DLZ Indiana, LLC
December 16, 2019

Abstract

Addendum to the Historic Property Short Report for the Vernal Pike Connector Project (17 February 2019)

City of Bloomington, Monroe and Richland Townships, Monroe County, Indiana

This report documents the identification and evaluation efforts for properties included in the revised Area of Potential Effects (APE) for the Vernal Pike Connector Project. Above-ground resources located within the project's revised APE were identified and evaluated in accordance with Section 106, National Historic Preservation Act (NHPA) of 1966, as amended, and the regulations implementing Section 106 (36 CFR Part 800).

As a result of the NHPA, as amended, and CFR Part 800, federal agencies are required to take into account the impact of federal undertakings upon historic properties in the area of the undertaking. Historic properties include buildings, structures, sites, objects, and/or districts that are eligible for or listed on the National Register of Historic Places (NRHP). As this project is receiving funding from the Federal Highway Administration (FHWA), it is subject to a Section 106 review.

The revised APE contains no properties listed in the NRHP. The revised APE contains no properties that are recommended eligible for listing in the NRHP.

- Determining, based upon its significance, which aspects of a property's integrity are particularly vital for being nominated.

Once a property is found to possess integrity, it must also satisfy at least one of the four following criteria, known as the Criteria for Evaluation:

- A. The property is associated with events that have made a significant contribution to the broad patterns of our history; or
- B. The property is associated with the lives of persons significant in our past; or
- C. The property embodies the distinctive characteristics of a type, period, or method of construction, or represents the work of a master, or possesses high artistic values, or represents a significant and distinguishable entity whose components may lack individual distinction; or
- D. The property may have yielded or may be likely to yield information important in prehistory or history.

To determine a property's significance it must be evaluated within its historic context. A property must be shown through scholarly research to have been extant at the time of its significance and to have played an important role within the theme(s) of its significance; mere association with the time period, historic event or themes, or important person is not enough to establish eligibility under Criterion A or B. Likewise, it is not sufficient for a property to be eligible under Criterion C based upon its rarity or because it was designed by a noted architect; it must also demonstrate significance.

Project areas are also evaluated for the potential presence of a historic district. A historic district is defined in the National Park Service's *National Register Bulletin 15: How to Apply the National Register Criteria for Evaluation*¹ as possessing "a significant concentration, linkage, or continuity or sites, buildings, structures, or objects united historically or aesthetically by plan or physical development." Like individual properties, districts must display significance, which is usually derived from its unified existence. For this reason a district may be composed of components which lack individual distinction. No part of the project area appears to possess a direct or specific correlation to a historic theme or display distinctive architectural significance that would set it apart and make it eligible as a historic district.

During the field reconnaissance the historian did not identify any buildings constructed before 1971. Photographs of all the properties in the APE can be found in Appendix A: Key Map and Project Site Photographs.

Conclusion

The revised APE contains no properties listed in the NRHP.

As a result of identification and evaluation efforts for this project, no resources are recommended eligible for listing in the NRHP.

¹ Available at http://www.nps.gov/nr/publications/bulletins/nrb15/nrb15_2.htm.

**Pages Excerpted from the Archaeological Short Report
(N.S. Services, LLC., 2018)**

And

**Pages Excerpted from the Addendum Archaeological Short
Report (N.S. Services, LLC., 2019)**



Vernal Pike Connector
From West Vernal Pike to Profile Parkway
Des. No.: 1702957 & 1900406
Monroe County, Indiana

Exhibit 7



INDIANA ARCHAEOLOGICAL SHORT REPORT

State Form 54566 (1-11)

INDIANA DEPARTMENT OF NATURAL RESOURCES DIVISION OF HISTORIC PRESERVATION AND ARCHAEOLOGY

402 West Washington Street, Room W274
Indianapolis, Indiana 46204-2739
Telephone Number: (317) 232-1646
Fax Number: (317) 232-0693
E-mail: dhpa@dnr.IN.gov

Where applicable, the use of this form is recommended but not required by the Division of Historic Preservation and Archaeology.

Author: Stacy N. Bennett and Jeffrey A. Plunkett

Date (month, day, year): January 11, 2019

Project Title: Phase Ia Archaeological Field Reconnaissance: Vernal Pike Connector Roadway Project in
Bloomington, Monroe County, Indiana (Des. No. 1702957)

PROJECT OVERVIEW

Project Description:

This project is for construction of the Vernal Pike Connector and Bridge over the existing CSX Railroad on new alignment. The typical roadway section will consist of two 12-foot lanes with standard concrete curb and gutter, along with a 5-foot sidewalk and a 10-foot multiuse path on either side. A 3-foot bike lane may also be added along Sunrise Greetings Court. The total length of this project is approximately 0.45 miles. The project may include retaining walls at the fill slopes of the railroad overpass bridge to optimize earth embankment, cost and right-of-way. ADA accessible curb ramps will be constructed at all public street crossings and ADA compliant drive approaches at all private entrances.

INDOT Designation Number/ Contract Number: Des. No. 1702957

Project Number: 18353

DHPA Number:

Approved DHPA Plan Number:

Prepared For: DLZ Indiana, LLC

Contact Person: Daniel Stevens

Address: 2211 E. Jefferson Blvd.

City: South Bend

State: IN

ZIP Code: 46615-2692

Telephone Number: (574) 236-4400

Email Address: dstevens@dlz.com

Principal Investigator: Jeffrey A. Plunkett

Signature:

Company/Institution: NS Services, LLC

Address: 4974 S. Cobblestone Drive

City: Zionsville

State: IN

ZIP Code: 46077

Telephone Number: (317) 773-2774

Email Address: j.plunkett@nsenvservices.com

Comments:

A typical soil profile for shovel probes excavated within the wooded portions of the project area consisted of approximately 15 cm of dark brown (10YR3/3) silt loam over a dark yellowish brown (10YR4/6) silt loam.

RESULTS

- ☐ Archaeological records check has determined that the project area does not have the potential to contain archaeological resources.
- ☐ Archaeological records check has determined that the project area has the potential to contain archaeological resources.
- ☒ Phase Ia reconnaissance has located no archaeological resources in the project area.
- ☐ Phase Ia reconnaissance has identified landforms conducive to buried archaeological deposits.

Actual Area Surveyed hectares:

02.1

acres:

05.3

Comments:

RECOMMENDATION

- ☐ The archaeological records check has determined that the project area has the potential to contain archaeological resources and a Phase Ia archaeological reconnaissance is recommended.
- ☐ The archaeological records check has determined that the project area does not have the potential to contain archaeological resources and no further work is recommended before the project is allowed to proceed.
- ☒ The Phase Ia archaeological reconnaissance has located no archaeological sites within the project area and it is recommended that the project be allowed to proceed as planned.

☐ The Phase Ia archaeological reconnaissance has determined that the project area includes landforms which have the potential to contain buried archaeological deposits. It is recommended that Phase Ic archaeological subsurface reconnaissance be conducted before the project is allowed to proceed.

☐ The Phase Ia archaeological reconnaissance has determined that the project area is within 100 feet of a cemetery and a Cemetery Development Plan is required per IC-14-21-1-26.5.

Cemetery Name:

Other Recommendations/Commitments:

Pursuant to IC-14-21-1, if any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646.

ATTACHMENTS

- ☒ Figure showing project location within Indiana.
- ☒ USGS topographic map showing the project area (*1:24,000 scale*).
- ☒ Aerial photograph showing the project area, land use, and survey methods.
- ☒ Photographs of the project area.
- ☐ Project plans (*if available*)



INDIANA ARCHAEOLOGICAL SHORT REPORT

State Form 54566 (1-11)

INDIANA DEPARTMENT OF NATURAL RESOURCES DIVISION OF HISTORIC PRESERVATION AND ARCHAEOLOGY

402 West Washington Street, Room W274
Indianapolis, Indiana 46204-2739
Telephone Number: (317) 232-1646
Fax Number: (317) 232-0693
E-mail: dhpa@dnr.IN.gov

Where applicable, the use of this form is recommended but not required by the Division of Historic Preservation and Archaeology.

Author: Jeffrey A. Plunkett

Date (month, day, year): November 18, 2019

Project Title: Phase Ia Archaeological Field Reconnaissance for an Addendum to the Vernal Pike Connector Roadway Project in Bloomington, Monroe County, Indiana (Des. No. 1702957)

PROJECT OVERVIEW

Project Description:

This project is for construction of the Vernal Pike Connector and Bridge over the existing CSX Railroad on new alignment. The typical roadway section will consist of two 12-foot lanes with standard concrete curb and gutter, along with a 5-foot sidewalk and a 10-foot multiuse path on either side. A 3-foot bike lane may also be added along Sunrise Greetings Court. The total length of this project is approximately 0.45 miles. The project may include retaining walls at the fill slopes of the railroad overpass bridge to optimize earth embankment, cost and right-of-way. ADA accessible curb ramps will be constructed at all public street crossings and ADA compliant drive approaches at all private entrances.

INDOT Designation Number/ Contract Number: Des. No. 1702957

Project Number: 18353a

DHPA Number:

Approved DHPA Plan Number:

Prepared For: DLZ Indiana, LLC

Contact Person: Daniel Stevens

Address: 2211 E. Jefferson Blvd.

City: South Bend

State: IN

ZIP Code: 46615-2692

Telephone Number: (574) 236-4400

Email Address: dstevens@dlz.com

Principal Investigator: Jeffrey A. Plunkett

Signature: Jeffrey Plunkett

Digitally signed by Jeffrey Plunkett
DN: cn=Jeffrey Plunkett, o=NS Services, LLC, ou,
email=j.plunkett@nsservices.com, c=US
Date: 2019.11.18 22:35:23 -0500

Company/Institution: NS Services, LLC

Address: 4974 S. Cobblestone Drive

City: Zionsville

State: IN

ZIP Code: 46077

Telephone Number: (317) 773-2774

Email Address: j.plunkett@nsservices.com

Describe Disturbances:

development, the construction of Sunrise Greeting Court, and the installation of various buried utilities. The southern portion of the project area contains railroad tracks, which have been in existence in this location for well over 100 years. In addition, portions of the project area located south of these existing railroad tracks were also found to have been disturbed by commercial and industrial development, the construction of Gates Drive, and the installation of various buried utilities.

Comments:

A typical soil profile for shovel probes excavated within the project area consisted of approximately 15 cm of dark brown (10YR3/3) silt loam over a dark yellowish brown (10YR4/6) silt loam.

RESULTS

- ☐ Archaeological records check has determined that the project area does not have the potential to contain archaeological resources.
- ☐ Archaeological records check has determined that the project area has the potential to contain archaeological resources.
- ☒ Phase Ia reconnaissance has located no archaeological resources in the project area.
- ☐ Phase Ia reconnaissance has identified landforms conducive to buried archaeological deposits.

Actual Area Surveyed hectares:

03.2

acres:

08.0

Comments:

RECOMMENDATION

- ☐ The archaeological records check has determined that the project area has the potential to contain archaeological resources and a Phase Ia archaeological reconnaissance is recommended.
- ☐ The archaeological records check has determined that the project area does not have the potential to contain archaeological resources and no further work is recommended before the project is allowed to proceed.
- ☒ The Phase Ia archaeological reconnaissance has located no archaeological sites within the project area and it is recommended that the project be allowed to proceed as planned.

- ☐ The Phase Ia archaeological reconnaissance has determined that the project area includes landforms which have the potential to contain buried archaeological deposits. It is recommended that Phase Ic archaeological subsurface reconnaissance be conducted before the project is allowed to proceed.

- ☐ The Phase Ia archaeological reconnaissance has determined that the project area is within 100 feet of a cemetery and a Cemetery Development Plan is required per IC-14-21-1-26.5.

Cemetery Name:

Other Recommendations/Commitments:

Pursuant to IC-14-21-1, if any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646.

ATTACHMENTS

- ☒ Figure showing project location within Indiana.
- ☒ USGS topographic map showing the project area (1:24,000 scale).

Initial Consultation Letter and Consulting Party Correspondence



Vernal Pike Connector
From West Vernal Pike to Profile Parkway
Des. No.: 1702957 & 1900406
Monroe County, Indiana

Exhibit 8

Daniel Stevens

From: Daniel Stevens
Sent: Thursday, May 09, 2019 10:07 AM
To: commissionersoffice@co.monroe.in.us; jchambers@co.monroe.in.us; glmurray@indiana.edu; director@monroehistory.org; martipa@bloomington.in.gov; sburgess@indianalandmarks.org; central@indianalandmarks.org; BRI@bloomingtonrestorations.org; downtown@kiva.net; tcoppock@downtownbloomington.com; jnester@co.monroe.in.us; preserve@bluemarble.com; herteric@bloomington.in.gov; vandevej@bloomington.in.gov
Cc: cmoffatt@indot.in.gov; aross3@indot.in.gov; michelle.allen@dot.gov; Miller, Shaun
Subject: FHWA Project: Des. No. 1702957 & 1900406; Vernal Pike Connector Project, Monroe County, Indiana

Des. No.: 1702957 & 1900406

Project Description: Vernal Pike Connector Project

Location: Monroe and Richland Townships, Monroe County, Indiana

Monroe County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation, proposes to proceed with the extension of Sunrise Greeting Court south to Profile Parkway, connecting West Vernal Pike to Profile Parkway via a railroad overpass (Des. No. 1702957 & 1900406).

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

- Monroe County Board of Commissioners
- Monroe County Highway Superintendent
- Monroe County Historian
- Monroe County History Center
- Bloomington/Monroe County MPO
- Indiana Landmarks Central Regional Office
- Bloomington Restorations, Inc.
- Downtown Bloomington Commission
- Monroe County Historic Preservation Board of Review
- Preservation Development, Inc.
- City of Bloomington, Historic Preservation Program Manager, Department of Housing and Neighborhood Development
- City of Bloomington Street Department
- Delaware Tribe of Indians, Oklahoma
- Eastern Shawnee Tribe of Oklahoma
- Miami Tribe of Oklahoma
- Peoria Tribe of Indians of Oklahoma
- Pokagon Band of Potawatomi Indians

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the letter, historic property report, and archaeology report (Tribes only) located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. If we do not receive a response from an invited consulting party in the time allotted, the project will proceed consistent with the proposed design. **Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.**

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.
Thank you in advance for your input,

Daniel Stevens | Environmental Scientist

574-236-4400 x614 (office) | 574-514-8266 (cell)
dstevens@dlz.com | www.dlz.com



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Daniel Stevens

From: Ross, Anthony <ARoss3@indot.IN.gov>
Sent: Thursday, May 09, 2019 10:14 AM
To: thpo@estoo.net; 'dhunter@miamination.com'; lpappenfort@peoriatribe.com; Matthew Bussler (Matthew.Bussler@pokagonband-nsn.gov); lheady@delawaretribe.org
Cc: Moffatt, Charles D; Miller, Shaun (INDOT); Branigin, Susan; Kumar, Anuradha; Allen, Michelle (FHWA); Daniel Stevens; Candace Hudziak; Hicks, Zachary; ljridge@co.monroe.in.us; j.plunkett@nsenvservices.com
Subject: FHWA Project: Des. No. 1702957 & 1900406; Vernal Pike Connector Project, Monroe County, Indiana
Attachments: VernalPike_Des1702957_ECL_2019-5-6.pdf

Des. No.: 1702957 & 1900406

Project Description: Vernal Pike Connector Project

Location: Monroe and Richland Townships, Monroe County, Indiana

Monroe County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation, proposes to proceed with the extension of Sunrise Greeting Court south to Profile Parkway, connecting West Vernal Pike to Profile Parkway via a railroad overpass (Des. No. 1702957 & 1900406).

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

- Monroe County Board of Commissioners
- Monroe County Highway Superintendent
- Monroe County Historian
- Monroe County History Center
- Bloomington/Monroe County MPO
- Indiana Landmarks Central Regional Office
- Bloomington Restorations, Inc.
- Downtown Bloomington Commission
- Monroe County Historic Preservation Board of Review
- Preservation Development, Inc.
- City of Bloomington, Historic Preservation Program Manager, Department of Housing and Neighborhood Development
- City of Bloomington Street Department
- Delaware Tribe of Indians, Oklahoma
- Eastern Shawnee Tribe of Oklahoma
- Miami Tribe of Oklahoma
- Peoria Tribe of Indians of Oklahoma
- Pokagon Band of Potawatomi Indians

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the letter, historic property report, and archaeology report (Tribes only) located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. If we do not receive a response from an invited consulting party in the time allotted, the project will proceed consistent with the proposed design. **Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.**

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

Anthony Ross, Ph.D.

Historian

Cultural Resources Office

Environmental Services

100 N. Senate Ave., Room N642-ES

Indianapolis, IN 46204

Office: (317) 234-0142

Email: aross3@indot.in.gov



**** Historic Property Report (HPR) guidelines can be found [here](#)**



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

Eric Holcomb, Governor
Joe McGuinness, Commissioner

May 6, 2019

This letter was sent to the listed parties.

RE: Vernal Pike Connector Project
Monroe County, Indiana
Des. No. 1702957 and 1900406

DLZ. No.: 1863-2010-90

Dear Consulting Party,

Monroe County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with the Vernal Pike Connector Project which includes the extension of Sunrise Greeting Court south to Profile Parkway, connecting West Vernal Pike to Profile Parkway via a railroad overpass (Des. No. 1702957 and 1900406). DLZ Indiana, LLC is under contract with Monroe County to advance the environmental documentation for the referenced project.

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

The proposed Vernal Pike Connector project will include road and bridge construction on a new alignment over CSX Railroad (operated by Indiana Railroad). The project is within Sections 25 and 36, Township 9 North, Range 2 West, Richland Township, USGS Bloomington Topographic Quadrangle, Monroe County, Indiana.

The project limits begin approximately 500 feet west and 100 feet east of the intersection of West Vernal Pike and Sunrise Greetings Court, then proceed south along Sunrise Greetings Court for approximately 830 feet until reaching the cul-de-sac, then extend approximately 1,500 feet to the southeast on new alignment and cross over the CSX Railroad and continue until joining Profile Parkway (currently under development with construction being completed prior to this project).

The project is necessary to provide a connection between the existing commercial and industrial developments in the project vicinity. As a result of the I-69 project, access to the area from SR-37-I-69 was cut off at West Vernal Pike and at West Whitehall Crossing Boulevard. Consequently, access to the existing commercial development, west of I-69, is now via Gates Drive only. One of the main connectivity issues is for traffic coming from the north and trying to access the commercial development immediately to the west of I-69 and north of 3rd Street, while also avoiding the at-grade rail crossings on Curry Pike. Additionally, traffic coming from the northwest on 17th Street/Vernal Pike would have to either deal with the rail crossings on Curry Pike or proceed down to 3rd Street east of I-69, which is difficult because there is no direct north-south road between 17th and 3rd. Street. In order to improve access and connectivity, Monroe County is currently in the process of designing the Profile Parkway extension (east of Curry Pike to Gates Drive). This is a locally funded project

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An Equal Opportunity Employer



that will serve the future development along Profile Parkway and is expected to be completed prior to the construction of the proposed Vernal Pike Connector project. The Vernal Pike Connector project, which includes the extension of Sunrise Greeting Court and railroad overpass, will connect to the Profile Parkway extension.

The proposed project includes the extension of Sunrise Greeting Court south to Profile Parkway (which is currently under design), connecting West Vernal Pike to Profile Parkway via a railroad overpass. The project will include road and bridge construction on a new alignment over CSX Railroad (operated by Indiana Railroad), with a total project length of approximately 0.45 miles. The typical roadway section will consist of two 12-foot lanes with 8-inch concrete curb and gutter, a 5-foot sidewalk on the west side and a 10-foot Multiuse path on the east side of the proposed roadway, along with 3-foot bike lanes adjacent to the travel lanes. Retaining walls may be incorporated at the fill slopes of the railroad overpass bridge to optimize earth embankment cost and right-of-way. The proposed road construction will tie into the existing roadway section of Sunrise Greeting Court at the existing Cul-de-Sac, approximately 830 feet south of West Vernal Pike. Construction of the 10-foot Multiuse path will continue along the east side of Sunrise Greeting Court and will extend to West Vernal Pike. An eastbound right turn lane from West Vernal Pike to Sunrise Greeting Court may also be constructed along with a new traffic signal at the intersection of West Vernal Pike and Sunrise Greeting Court. The anticipated project right-of-way need is approximately 5 acres. The project area north of the railroad tracks will be connected to existing storm sewers. The project area south of the railroad tracks will be connected to the new Profile Parkway storm sewer system.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. In accordance with 36 CFR 800.2 (c), you are hereby requested to be a consulting party to participate in the Section 106 process. Entities that have been invited to participate in the Section 106 consultation process for this project are identified in the attached list. Per 36 CFR 800.3(f), we hereby request that the Indiana State Historic Preservation Officer (SHPO) notify this office if the SHPO staff is aware of any other parties that may be entitled to be consulting parties or should be contacted as potential consulting parties for the project.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/digital-library-section-106-landing/citizens-guide-section-106-review>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains no resources listed in the National Register of Historic Places (NRHP). A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the APE for potential eligibility for the NRHP. As a result of the historic property identification and evaluation efforts, no above-ground resources are recommended as eligible for listing in the NRHP.

With regards to archaeological resources, an archaeologist who meets the Secretary of the Interior's Professional Qualification Standards conducted a Phase 1a archeological survey of the project area. As a result of these efforts, no NRHP-listed or NRHP-eligible archeological sites were identified and no further work is recommended.

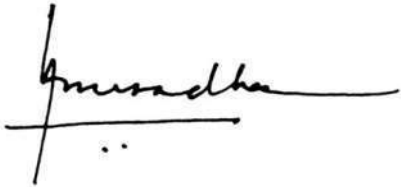
The Historic Property Report and Archaeology Report (Tribes only) are available for review in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If you prefer a hard copy of this material, please respond to this email with your request within seven (7) days.

Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party, or if you do not respond, you will not be included on the list of consulting parties for this project. If we do not receive your response in the time allotted, the project will proceed consistent with the proposed design and you will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Dan Stevens of DLZ by telephone (574-236-4400) or by email (dstevens@dlz.com). All future responses regarding the proposed project should be forwarded to DLZ at the following address: Dan Stevens, Environmental Scientist, DLZ Indiana, LLC, 2211 East Jefferson Boulevard, South Bend, Indiana 46615 (dstevens@dlz.com).

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Sincerely,



Anuradha V. Kumar, Manager
Cultural Resources Office
Environmental Services

Enclosures:

- Figure 1 - Location Map
- Figure 2 -Topographic Map
- Figure 3 -Aerial Photograph
- Figure 4 -Site Photographs

In addition to the SHPO, this consultation request is being sent to the following agencies/parties:

Monroe County Board of Commissioners
100 W. Kirkwood Ave.
Bloomington, IN 47404
commissionersoffice@co.monroe.in.us

Monroe County Highway Superintendent
Highway Garage
5900 W Foster Curry Dr.
Bloomington, IN 47403
jchambers@co.monroe.in.us

Monroe County Historian
Glenda Murray
525 West 3rd Street
Bloomington, IN 47404
(812) 332-6268
glmurray@indiana.edu

Monroe County History Center
202 E. 6th St.
Bloomington, IN 47408-3518
(812) 332-2517
President: Dave Musgrave
director@monroehistory.org

Bloomington/Monroe County
Metropolitan Planning Organization
401 N. Morton St.
Suite 130
P.O. Box 100
Bloomington, IN 47402
Phone: 812.349.3423
Fax: 812.349.3535
MPO Transportation Planner: Patrick Martin
martipa@bloomington.in.gov

Indiana Landmarks Central Regional Office
1201 Central Avenue
Indianapolis, IN 46202-3204
317-639-4534 or 800-450-4534
central@indianalandmarks.org, and
sburgess@indianalandmarks.org

Bloomington Restorations, Inc.
Elizabeth Cox-Ash
2920 East Tenth Street
Bloomington, IN 47408
BRI@bloomingtonrestorations.org

Downtown Bloomington Commission
Talisha Coppock, Director
302 South College Avenue

Bloomington, IN 47303
tcoppock@downtownbloomington.com, and
downtown@kiva.net

Monroe County Historic Preservation Board of Review
Jackie Nester, Planner
Monroe County Government Center
Bloomington, IN 47404
jnester@co.monroe.in.us

Duncan Campbell
Preservation Development, Inc.
218 North Rogers St.
Bloomington IN 47404
812-336-2065
preserve@bluemarble.com

City of Bloomington Historic Preservation Program Manager
Department of Housing and Neighborhood Development
Conor Herterich
(812)-349-3507
herteric@bloomington.in.gov

City of Bloomington Street Department
1981 S Henderson Bloomington IN 47401
vandevj@bloomington.in.gov

Delaware Tribe of Indians, Oklahoma

Eastern Shawnee Tribe of Oklahoma

Miami Tribe of Oklahoma

Peoria Tribe of Indians of Oklahoma

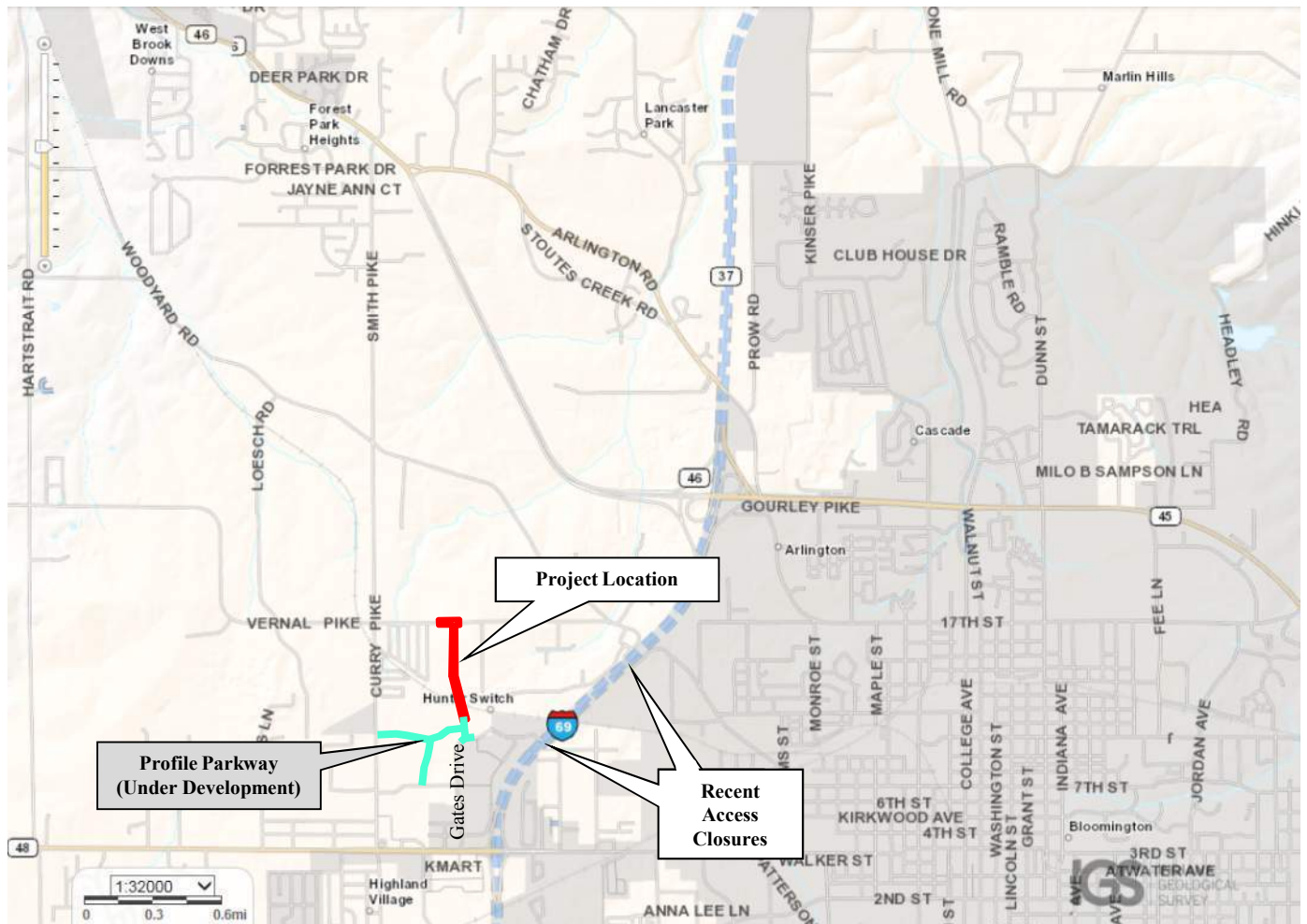
Pokagon Band of Potawatomi Indians

Monroe County



Location Map

Project Location



<http://maps.indiana.edu/>

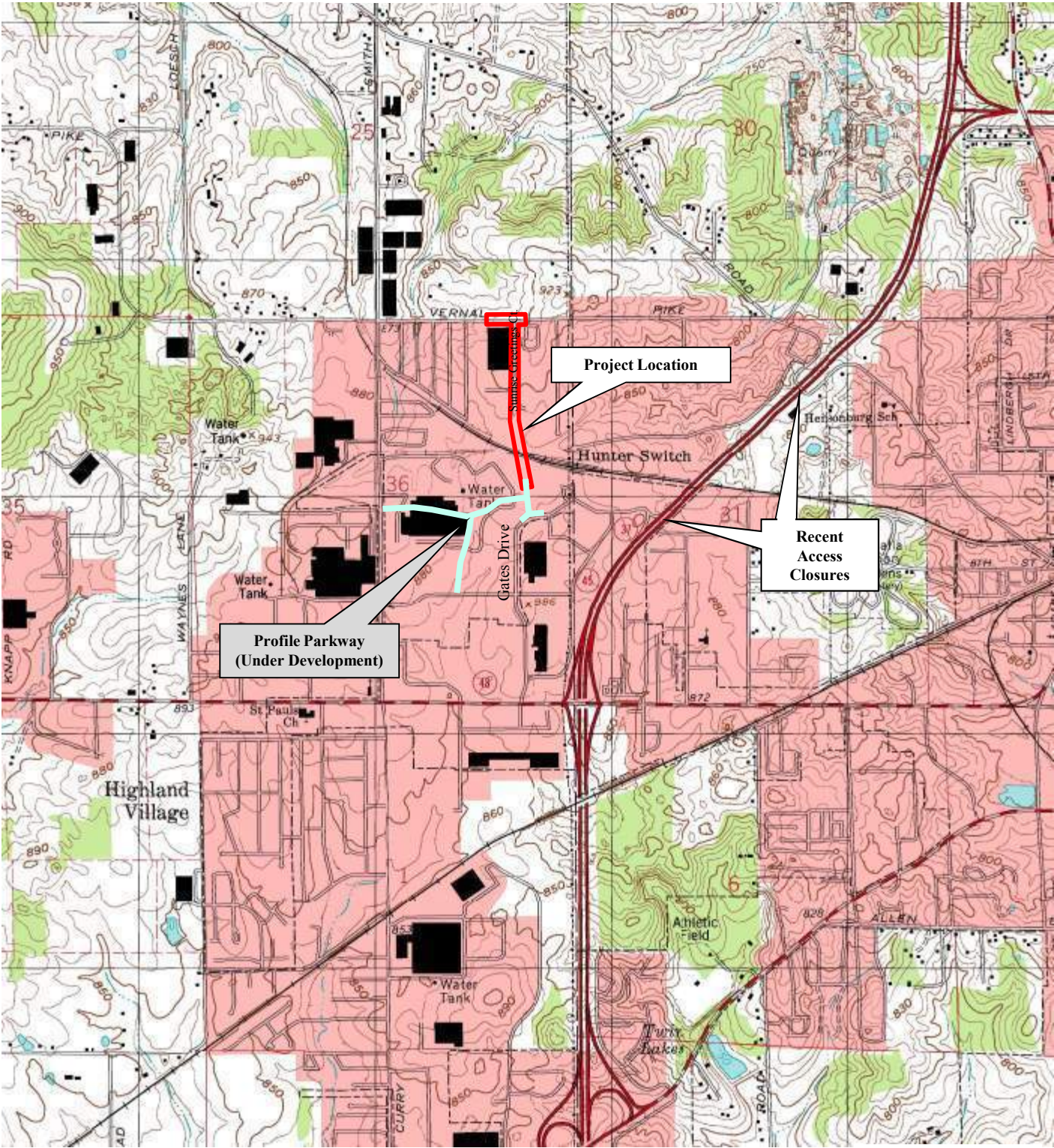


Vernal Pike Connector
From West Vernal Pike to Profile Parkway
Des. No.: 1702957 & 1900406
Monroe County, Indiana

Scale: NTS

Figure: 1

USGS Quadrangle Map



USGS Bloomington Quadrangle Map

	Vernal Pike Connector From West Vernal Pike to Profile Parkway Des. No.: 1702957 & 1900406 Monroe County, Indiana	Scale: 1"=2000'
		Figure: 2

Aerial



Vernal Pike Connector
From West Vernal Pike to Profile Parkway
Des. No.: 1702957 & 1900406
Monroe County, Indiana

Scale: See Map

Figure: 3

Daniel Stevens

From: Patrick Martin <martipa@bloomington.in.gov>
Sent: Thursday, May 09, 2019 3:14 PM
To: Daniel Stevens
Cc: Ryan Clemens; Beth Rosenbarger
Subject: Re: FHWA Project: Des. No. 1702957 & 1900406; Vernal Pike Connector Project, Monroe County, Indiana

Good Afternoon Daniel,

Thank you for your early coordination letter regarding the Vernal Pike Connector Project DES#1702957 & 1900406.

The proposed projects are wholly consistent with the Bloomington-Monroe County 2040 Metropolitan Transportation Plan, the FY 2018-2021 Transportation Improvement Program, and the Draft FY 2020-2024 Improvement Program, all of which completed review and approvals by the BMCMPPO Committee structure.

Please contact either Ryan Clemens or myself at your earliest convenience if you should have any questions or require any additional information.

Thanks,

Pat Martin

On Thu, May 9, 2019 at 10:07 AM Daniel Stevens <dstevens@dlz.com> wrote:

Des. No.: 1702957 & 1900406

Project Description: Vernal Pike Connector Project

Location: Monroe and Richland Townships, Monroe County, Indiana

Monroe County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation, proposes to proceed with the extension of Sunrise Greeting Court south to Profile Parkway, connecting West Vernal Pike to Profile Parkway via a railroad overpass (Des. No. 1702957 & 1900406).

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

**MONROE COUNTY HISTORIC PRESERVATION
BOARD OF REVIEW**

501 N. Morton Street, Suite 224, Bloomington, IN 47404

Telephone: (812)-349-2560 / Fax: (812)-349-2967

www.co.monroe.in.us/tsd/Government/Infrastructure/PlanningDepartment/HistoricPreservation.aspx

May 29, 2019

Mr. Daniel Stevens
Environmental Scientist
DLZ

Dr. Mr. Stevens,

I am writing on behalf of the Monroe County Historic Preservation Board of Review, a Certified Local Government, regarding the FHWA Project: Des. No. 1702957 & 1900406; Vernal Pike Connector Project, Monroe County, Indiana. Thank you for the invitation to be a consulting party on the project.

The board has reviewed the reports generated as part of the Section 106 process and examined the project area. We concur with the reports and agree to a finding of 'no adverse affect' to historic properties.

Sincerely,

Danielle Bachant-Bell

Danielle Bachant-Bell, M.F.A. Historic Preservation
Chair, Monroe County Historic Preservation Board of Review

cc: Jackie Nester, Monroe County Planning Department



June 6, 2019

Dan Stevens
Environmental Scientist
DLZ Indiana, LLC
2211 East Jefferson Blvd.
South Bend, Indiana 46615
dstevens@dlz.com

Re.: Vernal Pike Connector Project Monroe County, Indiana Des. No. 1702957 and 1900406

Dear Mr. Stevens:

Thank you for the opportunity to comment on the above undertaking. Indiana Landmarks agrees to be a consulting party for the project.

Per the Historic Properties Report (HPR), we concur with the historian's finding that there are no properties listed in or eligible for listing in the National Register of Historic Places (NRHP) within the proposed Area of Potential Effects (APE) for the project.

However, we wonder if it might be more appropriate to expand the APE to include Monroe County's directly related Profile Parkway extension in its entirety. Although the undertaking denoted as the Profile Parkway extension is locally funded, its purpose seems to be virtually identical with the purpose of the Vernal Pike Connector Project, namely, to "provide a connection between the existing commercial and industrial developments in the project vicinity." Moreover, the Vernal Pike Connector Project could not fulfill its stated purpose without the concurrent completion of the Profile Parkway extension. For these reasons, we believe that it might be more proper to classify the Profile Parkway extension and the Vernal Pike Connector as two interdependent segments of the same project. If so, we believe that it might be most appropriate for the entire project to be subject to the Section 106 process, even if one of the segments does not directly receive federal funding. Accordingly, we recommend considering enlarging the APE to include the entire Profile Parkway extension and the "geographic area or areas within which [the] undertaking may directly or indirectly cause alterations in the character or use of historic properties."

We appreciate your consideration and will look forward to remaining involved in the Section 106 process for this project.

Sincerely,

Sam Burgess
Community Preservation Specialist



Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



June 12, 2019

Dan Stevens
Environmental Scientist
DLZ Indiana, LLC
2211 East Jefferson Boulevard
South Bend, Indiana 46615

Federal Agency: Indiana Department of Transportation ("INDOT"),
on behalf of Federal Highway Administration, Indiana Division ("FHWA")

Re: Early coordination letter, historic property short report (Hudziak, 2/17/2019), and Indiana archaeological short report (Bennett/Plunkett, 01/11/2019), for the proposed Vernal Pike Connector Project, in Bloomington and Richland Townships, Monroe County, Indiana (Des. Nos. 1702957 and 1900406; DHPA No. 23905)

Dear Mr. Stevens:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the "Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO" or "INDNR-DHPA") has reviewed INDOT's May 6, 2019, early coordination letter, with the aforementioned reports enclosed, all of which we received on May 14, 2019.

We are not aware of any parties who should be invited to participate in the Section 106 consultation on this project, beyond those whom INDOT already has invited.

The area of potential effects ("APE") proposed in the historic property short report ("HPSR"; Hudziak, 2/17/2019) appears to be of appropriate size to encompass the geographic area in which direct or indirect effects of this project could occur.

Based on the information and analysis in the HPSR, we agree that there do not appear to be any individual, above-ground properties or historic districts within the APE that would be eligible for inclusion in the National Register of Historic Places ("NRHP"). However if any other consulting party makes a reasoned argument, in a timely fashion, for eligibility of a property, then further consultation among the consulting parties would be necessary.

Additionally, in terms of potential impact on archaeological resources, based on the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the proposed project area; and we concur with the opinion of the archaeologist, as expressed in the Indiana archaeological short report (Bennett/Plunkett, 01/11/2019), that no further archaeological investigations appear necessary at the proposed project area.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery be reported to INDNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

Unless another consulting party has raised an eligibility issue about a property within the APE, it might now be appropriate to ask INDOT for a finding.

The archaeological reviewer for this project on the Indiana SHPO staff is Wade T. Tharp, and the structures reviewer is Danielle Kauffmann. However, if you have questions about our comments or about a procedural issue, please contact initially an INDOT Cultural Resources staff member who is assigned to this project.

In all future correspondence regarding the proposed Vernal Pike Connector Project in Bloomington and Richland Townships, Monroe County, Indiana (Des. No. 1702957 and 1900406), please refer to DHPA No. 23905.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:DMK:WTT:wt

emc: Michelle Allen, FHWA
Anuradha Kumar, INDOT
Shaun Miller, INDOT
Susan Branigin, INDOT
Shirley Clark, INDOT
Dan Stevens, DLZ Indiana, LLC
Wade T. Tharp, INDNR-DHPA
Danielle Kauffmann, INDNR-DHPA

**Additional Consultation Letter and
Consulting Party Correspondence**

	<p>Vernal Pike Connector From West Vernal Pike to Profile Parkway Des. No.: 1702957 & 1900406 Monroe County, Indiana</p>	<p>Exhibit 9</p>
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Daniel Stevens

From: Daniel Stevens
Sent: Thursday, January 2, 2020 1:53 PM
To: 'commissionersoffice@co.monroe.in.us'; 'jchambers@co.monroe.in.us'; 'glmurray@indiana.edu'; 'director@monroehistory.org'; 'martipa@bloomington.in.gov'; 'sburgess@indianalandmarks.org'; 'central@indianalandmarks.org'; 'BRI@bloomingtonrestorations.org'; 'downtown@kiva.net'; 'tcoppock@downtownbloomington.com'; 'jnester@co.monroe.in.us'; 'preserve@bluemarble.com'; 'herteric@bloomington.in.gov'; 'vandevej@bloomington.in.gov'
Cc: 'cmoffatt@indot.in.gov'; 'aross3@indot.in.gov'; 'michelle.allen@dot.gov'; Miller, Shaun
Subject: FHWA Project: Des. No. 1702957 & 1900406; Vernal Pike Connector Project, Monroe County, Indiana

Des. No.: 1702957 & 1900406

Project Description: Vernal Pike Connector Project

Location: Monroe and Richland Townships, Monroe County, Indiana

Monroe County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation, proposes to proceed with the extension of Sunrise Greeting Court south to Profile Parkway, connecting West Vernal Pike to Profile Parkway via a railroad overpass (Des. No. 1702957 & 1900406). The Section 106 Early Coordination Letter for this project was originally distributed on May 6, 2019.

As part of Section 106 of the National Historic Preservation Act, an Addendum Historic Property Report, Addendum Archaeology Report, and an additional consultation letter have been prepared and are ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

Daniel Stevens | Environmental Scientist

574-236-4400 x614 (office) | 574-514-8266 (cell)
dstevens@dlz.com | www.dlz.com



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Daniel Stevens

From: Ross, Anthony <ARoss3@indot.IN.gov>
Sent: Friday, January 3, 2020 9:00 AM
To: thpo@estoo.net; 'dhunter@miamination.com'; lpappenfort@peoriatribe.com; Matthew Bussler (Matthew.Bussler@pokagonband-nsn.gov); 'lheady@delawaretribe.org'; Kauffmann, Danielle M; Tharp, Wade
Cc: Allen, Michelle (FHWA); Miller, Shaun (INDOT); Moffatt, Charles D; Branigin, Susan; ljridge@co.monroe.in.us; Hicks, Zachary; Daniel Stevens; Candace Hudziak
Subject: FHWA Project: Des. No. 1702957 & 1900406; Vernal Pike Connector Project, Monroe County, Indiana

EXTERNAL: Message origin is from an external network. Use proper judgment and caution when opening attachments, clicking links, or responding to this email.

Des. No.: 1702957 & 1900406

Project Description: Vernal Pike Connector Project

Location: Monroe and Richland Townships, Monroe County, Indiana

Monroe County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation, proposes to proceed with the extension of Sunrise Greeting Court south to Profile Parkway, connecting West Vernal Pike to Profile Parkway via a railroad overpass (Des. No. 1702957 & 1900406). The Section 106 Early Coordination Letter for this project was originally distributed on May 6, 2019.

As part of Section 106 of the National Historic Preservation Act, an Addendum Historic Property Report, Addendum Archaeology Report, and an additional consultation letter have been prepared and are ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

Anthony Ross, Ph.D.

LPA Program Administrator

Cultural Resources Office

Environmental Services

100 N. Senate Ave., Room N642-ES

Indianapolis, IN 46204

Office: (317) 234-0142

Email: aross3@indot.in.gov





INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

Eric Holcomb, Governor
Joe McGuinness, Commissioner

January 2, 2020

This letter was sent to the listed parties.

RE: Vernal Pike Connector Project
Monroe County, Indiana
Des. No. 1702957 and 1900406
DHPA No. 23905

DLZ. No.: 1863-2010-90

Dear Consulting Party,

Monroe County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with the Vernal Pike Connector Project which includes the extension of Sunrise Greeting Court south to Profile Parkway, connecting West Vernal Pike to Profile Parkway via a railroad overpass (Des. No. 1702957 and 1900406).

A Section 106 early coordination letter was distributed on May 6, 2019. In addition, this letter notified consulting parties that a historic property report and archaeology report were available for review and comment.

The proposed Vernal Pike Connector project will include road and bridge construction on a new alignment over CSX Railroad (operated by Indiana Railroad). The project is within Sections 25 and 36, Township 9 North, Range 2 West, Richland Township, USGS Bloomington Topographic Quadrangle, Monroe County, Indiana.

Based on comments received on the May 6, 2019 early coordination, the southern project study limits have been extended approximately 550 feet south to North Gates Drive. This will incorporate a portion of the locally funded Profile Parkway project into the impact assessment for the Vernal Pike Connector Project. This portion of the Profile Parkway project is being included to provide the Vernal Pike Connector project with independent utility in case the locally funded Profile Parkway project becomes delayed or never constructed. The entire Profile Parkway project is not included since the Vernal Pike Connector does not rely on the completion of Profile Parkway to provide independent utility.

The revised project limits begin approximately 500 feet west and 100 feet east of the intersection of West Vernal Pike and Sunrise Greetings Court, then proceed south along Sunrise Greetings Court for approximately 830 feet until reaching the cul-de-sac, then extend approximately 1,500 feet to the southeast on new alignment and cross over the CSX Railroad and continue to Profile Parkway. The project then extends approximately 550 feet south along Profile Parkway to North Gates Drive. Profile Parkway is currently under development with construction being completed prior to this project.

The project is necessary to provide a connection between the existing commercial and industrial developments in the project vicinity. As a result of the I-69 project, access to the area from SR-37-I-69 was cut off at West Vernal Pike and at West Whitehall Crossing Boulevard. Consequently, access to the existing commercial development, west of I-69, is now via Gates Drive only. One of the main connectivity issues is for traffic coming from the north and trying to access the commercial development immediately to the west of I-69 and north of 3rd Street, while also avoiding the at-grade rail crossings on Curry Pike. Additionally, traffic coming from the northwest on 17th Street/Vernal Pike would have to either deal with the rail crossings on Curry Pike or proceed down to 3rd Street east of I-69, which is difficult because there is no direct north-south road between 17th and 3rd. Street. In order to improve access and connectivity, Monroe County is currently in the process of designing the Profile Parkway extension (east of Curry Pike to Gates Drive). This is a locally funded project that will serve the future development along Profile Parkway and is expected to be completed prior to the construction of the proposed Vernal Pike Connector project. The Vernal Pike Connector project, which includes the extension of Sunrise Greeting Court and railroad overpass, will connect to the Profile Parkway extension. The portion of the Profile Parkway project that will connect the Vernal Pike Connector to North Gates Drive is being included in the impact assessment for the Vernal Pike Connector Project.

The proposed project includes the extension of Sunrise Greeting Court south to Profile Parkway (which is currently under design), connecting West Vernal Pike to Profile Parkway and North Gates Drive via a railroad overpass. The project will include road and bridge construction on a new alignment over CSX Railroad (operated by Indiana Railroad), with a total project length of approximately 0.5 miles. The typical roadway section will consist of two 12-foot lanes with 8-inch concrete curb and gutter, a 5-foot sidewalk on the west side and a 10-foot multiuse path on the east side of the proposed roadway, along with 3-foot bike lanes adjacent to the travel lanes. Retaining walls may be incorporated at the fill slopes of the railroad overpass bridge to optimize earth embankment cost and right-of-way. The proposed road construction will tie into the existing roadway section of Sunrise Greeting Court at the existing cul-de-sac, approximately 830 feet south of West Vernal Pike. Construction of the 10-foot multiuse path will continue along the east side of Sunrise Greeting Court and will extend to West Vernal Pike. An eastbound right turn lane from West Vernal Pike to Sunrise Greeting Court may also be constructed along with a new traffic signal at the intersection of West Vernal Pike and Sunrise Greeting Court. The anticipated project right-of-way needed for the Vernal Pike Connector is approximately 2.72 acres of permanent and 1.04 acres of temporary right-of-way. An additional approximately 1.82 acres of permanent right-of-way is required for the included segment of Profile Parkway. The project area north of the railroad tracks will be connected to existing storm sewers. The project area south of the railroad tracks will be connected to the new Profile Parkway storm sewer system.

DLZ Indiana, LLC is under contract with Monroe County to advance the environmental documentation for the referenced project. H&H Associates, Inc (above-ground) and NS Services, Inc. (archaeology) have been subcontracted to complete the Section 106 documentation for the project.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, to assess the undertaking's effects and to seek ways to avoid, minimize, or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The expanded APE contains no resources listed in the National Register of Historic Places (NRHP).

A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the expanded APE for potential eligibility for the NRHP. As a result of the historic property identification and evaluation efforts, no above-ground resources are recommended as eligible for listing in the NRHP.

With regard to archaeological resources, an archaeologist who meets the Secretary of the Interior's Professional Qualification Standards identified no sites within the expanded project area. As a result of these efforts, no sites were recommended as eligible for listing in the NRHP and no further work is recommended.

In response to the May 6, 2019 early coordination letter, correspondence was received from the Bloomington/Monroe County MPO, Monroe County Historic Preservation Board of Review, Indiana Landmarks Central Regional Office, and SHPO. No other consulting party comments were received. Consulting Party correspondence is presented in Appendix A.

In a letter dated May 9, 2019 (Appendix A-1), the Bloomington/Monroe County MPO indicated that the proposed project is wholly consistent with the Bloomington-Monroe County 2040 Metropolitan Transportation Plan, the FY 2018-2021 Transportation Improvement Program, and the Draft FY 2020-2024 Improvement Program. No concerns regarding historic resources were raised by the MPO.

In a letter dated May 29, 2019 (Appendix A-2), the Monroe County Historic Board of Review concurred with the report's recommendations, and it agrees to a finding of no adverse effect to historic properties.

In a letter dated June 6, 2019 (Appendix A-3), the Indiana Landmarks Central Regional Office concurred that there are no properties listed in or eligible for listing in the NRHP within the proposed APE for the project. Indiana Landmarks also recommended considering enlarging the APE to including Profile Parkway, a locally funded project. In response to this comment, the APE has been enlarged to include the segment of Profile Parkway required to connect the Vernal Pike Connector to North Gates Drive. This was done to provide the Vernal Pike Connector project with independent utility in case the locally funded Profile Parkway project becomes delayed or never constructed. The entire Profile Project is not included since the Vernal Pike Connector does not rely on the completion of Profile Parkway to provide independent utility.

The SHPO provided comments dated July 12, 2019 (Appendix A-4 to A-5) indicating that the APE in the HPR appears to be of appropriate size to encompass the geographic area in which direct and indirect effects of this project could occur. The SHPO also agreed with the HPR's recommendations that there do not appear to be any individual, above-ground properties or historic districts within the APE that would be eligible for inclusion in the NRHP. The SHPO also concurred with the recommendations of the archeological report and agreed that no further archaeological investigations appear necessary at the proposed project area.

The Addendum Historic Property Report and Addendum Archaeology Report (Tribes only) are available for review in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and to respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be

completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If you prefer a hard-copy of this material, please respond to this email with your request within seven (7) days.

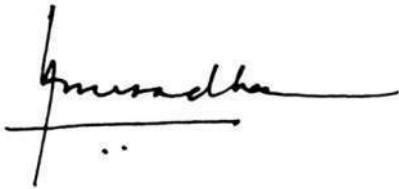
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party or if you have not previously accepted consulting party status and you do not respond to this letter, you will not be included on the list of consulting parties for this project and will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Daniel J. Stevens of DLZ by telephone (574-236-4400) or email (dstevens@dlz.com). All future responses regarding the proposed project should be forwarded to DLZ at the following address:

Daniel J. Stevens
Environmental Scientist
DLZ Indiana, LLC
2211 East Jefferson Boulevard
South Bend, Indiana 46615
dstevens@dlz.com.

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Sincerely,



Anuradha V. Kumar, Manager
Cultural Resources Office
Environmental Services

Enclosures:

Figure 1 - Location Map
Figure 2 - Topographic Map
Figure 3 - Aerial Photograph

Consulting Party Letters

Appendix A-1: Bloomington/Monroe County MPO
Appendix A-2: Monroe County Historic Board of Review
Appendix A-3: Indiana Landmarks Central Regional Office
Appendix A-4 to A-5: SHPO

NOTE: These Consulting Party letters have been removed from this Exhibit to avoid duplication since they are included in Exhibit 8

In addition to the SHPO, this consultation request is being sent to the following agencies/parties:

Monroe County Board of Commissioners
100 W. Kirkwood Ave.
Bloomington, IN 47404
commissionersoffice@co.monroe.in.us

Monroe County Highway Superintendent
Highway Garage
5900 W Foster Curry Dr.
Bloomington, IN 47403
jchambers@co.monroe.in.us

Monroe County Historian
Glenda Murray
525 West 3rd Street
Bloomington, IN 47404
(812) 332-6268
glmurray@indiana.edu

Monroe County History Center
202 E. 6th St.
Bloomington, IN 47408-3518
(812) 332-2517
President: Dave Musgrave
director@monroehistory.org

Bloomington/Monroe County
Metropolitan Planning Organization
401 N. Morton St.
Suite 130
P.O. Box 100
Bloomington, IN 47402
Phone: 812.349.3423
Fax: 812.349.3535
MPO Transportation Planner: Patrick Martin
martipa@bloomington.in.gov

Indiana Landmarks Central Regional Office
1201 Central Avenue
Indianapolis, IN 46202-3204
317-639-4534 or 800-450-4534
central@indianalandmarks.org, and
sburgess@indianalandmarks.org

Bloomington Restorations, Inc.
Elizabeth Cox-Ash
2920 East Tenth Street
Bloomington, IN 47408
BRI@bloomingtonrestorations.org

Downtown Bloomington Commission
Talisha Coppock, Director
302 South College Avenue
Bloomington, IN 47303
tcoppock@downtownbloomington.com, and
downtown@kiva.net

Monroe County Historic Preservation Board of
Review
Jackie Nester, Planner
Monroe County Government Center
Bloomington, IN 47404
jnester@co.monroe.in.us

Duncan Campbell
Preservation Development, Inc.
218 North Rogers St.
Bloomington IN 47404
812-336-2065
preserve@bluemarble.com

City of Bloomington Historic Preservation Program
Manager
Department of Housing and Neighborhood
Development
Conor Herterich
(812)-349-3507
herteric@bloomington.in.gov

City of Bloomington Street Department
1981 S Henderson Bloomington IN 47401
vandevej@bloomington.in.gov

Delaware Tribe of Indians, Oklahoma

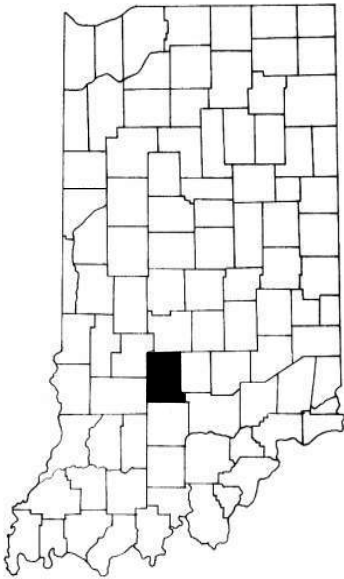
Eastern Shawnee Tribe of Oklahoma

Miami Tribe of Oklahoma

Peoria Tribe of Indians of Oklahoma

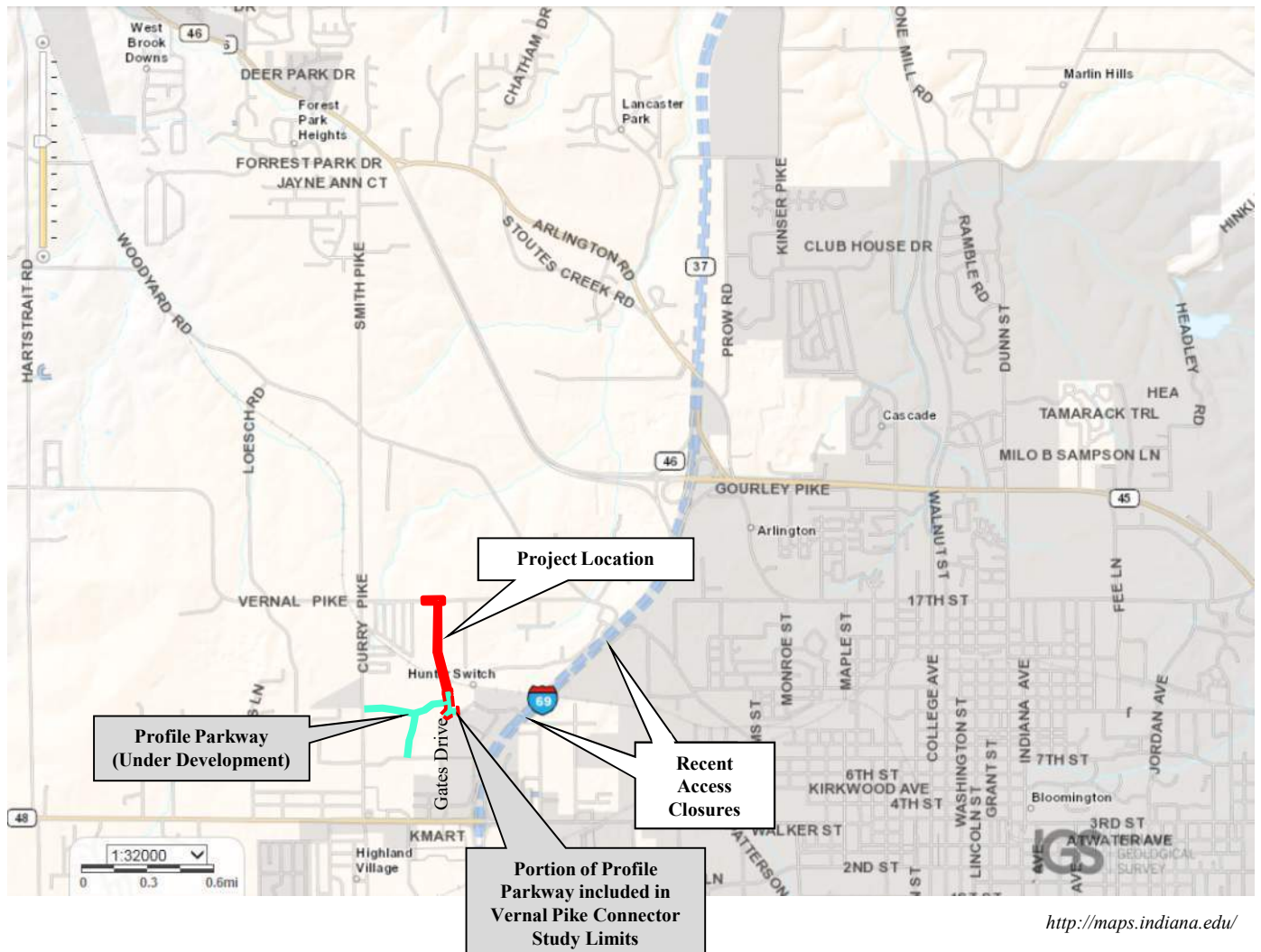
Pokagon Band of Potawatomi Indians

Monroe County



Location Map

Project Location



<http://maps.indiana.edu/>

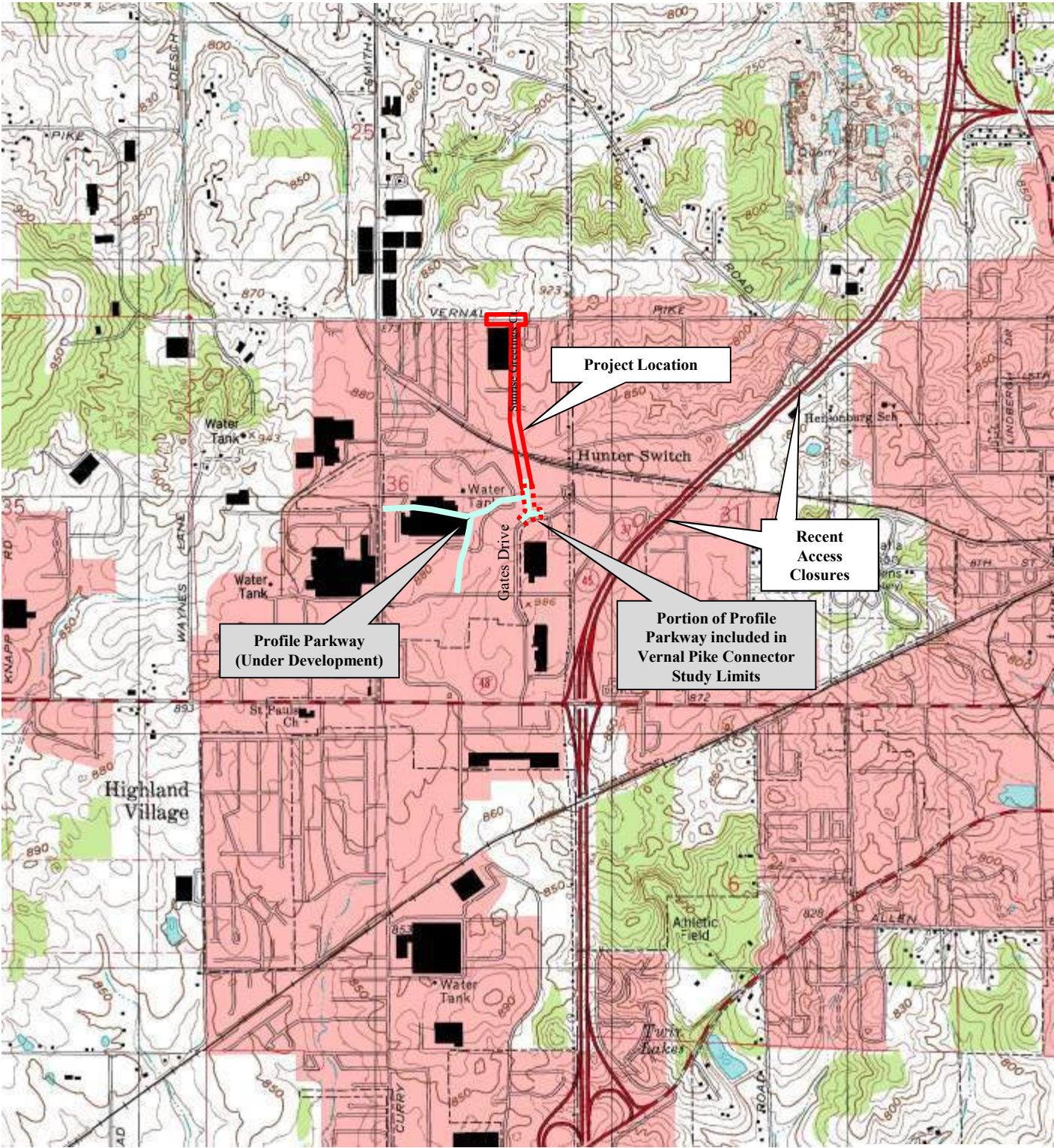


Vernal Pike Connector
From West Vernal Pike to Profile Parkway
Des. No.: 1702957 & 1900406
Monroe County, Indiana

Scale: NTS

Figure: 1

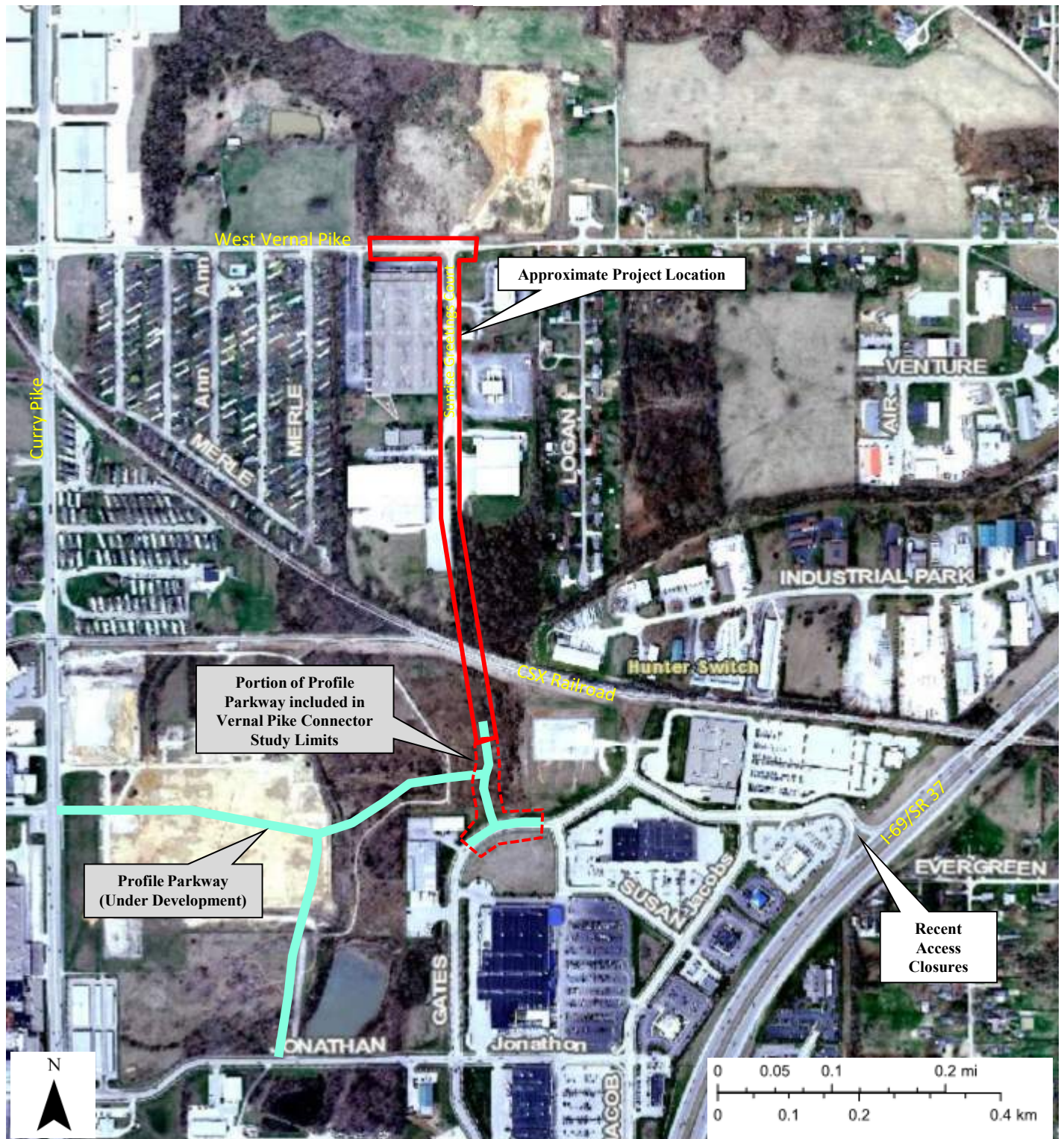
USGS Quadrangle Map



USGS Bloomington Quadrangle Map

	Vernal Pike Connector From West Vernal Pike to Profile Parkway Des. No.: 1702957 & 1900406 Monroe County, Indiana	Scale: 1"=2000'
		Figure: 2

Aerial



Vernal Pike Connector
From West Vernal Pike to Profile Parkway
Des. No.: 1702957 & 1900406
Monroe County, Indiana

Scale: See Map

Figure: 3



Miami Tribe of Oklahoma

3410 P St. NW, Miami, OK 74354 • P.O. Box 1326, Miami, OK 74355



Via email: smiller@indot.IN.gov

January 6, 2020

Shaun Miller
Archaeological Team Lead
Cultural Resources Office, Indiana DOT
575 North Pennsylvania Street
Indianapolis, IN 46204

Re: Des. No.: 1702957 & 1900406 Vernal Pike Connector Project – Comments of the Miami Tribe of Oklahoma

Dear Mr. Miller:

Aya, kikwehsitoole – I show you respect. My name is Diane Hunter, and I am the Tribal Historic Preservation Officer for the Federally Recognized Miami Tribe of Oklahoma. In this capacity, I am the Miami Tribe's point of contact for all Section 106 issues.

The Miami Tribe offers no objection to the above-mentioned project at this time, as we are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to the project site. However, as this project is within the aboriginal homelands of the Miami Tribe, if any human remains or Native American cultural items falling under the Native American Graves Protection and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of this project, the Miami Tribe requests immediate consultation with the entity of jurisdiction for the location of discovery. In such a case, please contact me at 918-541-8966 or by email at dhunter@miamination.com to initiate consultation.

The Miami Tribe accepts the invitation to serve as a consulting party to the proposed project. In my capacity as Tribal Historic Preservation Officer I am the point of contact for consultation.

Respectfully,

Diane Hunter
Tribal Historic Preservation Officer



Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhp@dnr.IN.gov · www.IN.gov/dnr/historic



February 6, 2020

Dan Stevens
Environmental Scientist
DLZ Indiana, LLC
2211 East Jefferson Boulevard
South Bend, Indiana 46615

Federal Agency: Indiana Department of Transportation ("INDOT"),
on behalf of Federal Highway Administration, Indiana Division ("FHWA")

Re: Revised project information, addendum historic property short report (Hudziak, 12/16/2019), and
addendum Indiana archaeological short report (Plunkett, 11/18/2019), for the proposed Vernal
Pike Connector Project, in Bloomington and Richland Townships, Monroe County, Indiana (Des.
Nos. 1702957 and 1900406; DHPA No. 23905)

Dear Mr. Stevens:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the "Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO" or "INDNR-DHPA") has reviewed INDOT's January 2, 2020, letter, with the aforementioned reports enclosed, all of which we received on January 7, 2020.

Thank you for providing updated information the proposed project. We appreciate the consideration taken to take into account the portion of the proposed Profile Parkway within the project area in case the locally-funded road project should be delayed or not constructed. The revised area of potential effects ("APE") proposed in the historic property short report addendum ("HPSR Addendum"; Hudziak, 12/16/2019) appears to be of appropriate size to encompass the geographic area in which direct or indirect effects of this project could occur.

Based on the information and analysis in the HPSR Addendum, we agree that there do not appear to be any individual, above-ground properties or historic districts within the APE that would be eligible for inclusion in the National Register of Historic Places ("NRHP").

Additionally, in terms of potential impact on archaeological resources, based on the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the additional portions of the proposed project area; and we concur with the opinion of the archaeologist, as expressed in the addendum Indiana archaeological short report (Plunkett, 11/18/2019), that no further archaeological investigations appear necessary at the additional portions of the proposed project area.

Furthermore, as previously indicated, in terms of potential impact on archaeological resources, based on the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the original portions of the proposed project area; and we concur with the opinion of the archaeologist, as expressed in the Indiana archaeological short report

(Bennett/Plunkett, 01/11/2019), that no further archaeological investigations appear necessary at the original portions of the proposed project area.

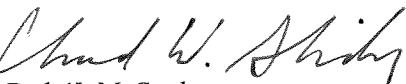
If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery be reported to INDNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

Unless another consulting party has raised an eligibility issue about a property within the APE, it might now be appropriate to ask INDOT for a finding.

The archaeological reviewer for this project on the Indiana SHPO staff is Wade T. Tharp, and the structures reviewer is Danielle Kauffmann. However, if you have questions about our comments or about a procedural issue, please contact initially an INDOT Cultural Resources staff member who is assigned to this project.

In all future correspondence regarding the proposed Vernal Pike Connector Project in Bloomington and Richland Townships, Monroe County, Indiana (Des. No. 1702957 and 1900406), please refer to DHPA No. 23905.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:DMK:WTT:wt

emc: Erica Tait, FHWA
Anuradha Kumar, INDOT
Shaun Miller, INDOT
Susan Branigin, INDOT
Shirley Clark, INDOT
Dan Stevens, DLZ Indiana, LLC
Candace Hudziak, H&H Associates, LLC
Patrick Martin, Bloomington/Monroe County MPO
Danielle Bachant-Bell, Monroe County Historic Preservation Board of Review
Joshua Biggs, Indiana Landmarks, Central Regional Office
Wade T. Tharp, INDNR-DHPA
Danielle Kauffmann, INDNR-DHPA

Des. 1702957 1900406

DLZ INDIANA, LLC.

(Governmental Unit)

.....Monroe County, Indiana

To: Herald Times
 1900 S. Walnut St.
 Bloomington, IN 47401

PUBLISHER'S CLAIM

LINE COUNT

Display Master (Must not exceed two actual lines, neither of which shall total more than four solid lines of the type in which the body of the advertisement is set) -- number of equivalent lines

Head -- number of lines

Body -- number of lines

Tail -- number of lines

Total number of lines in notice.....96

COMPUTATION OF CHARGES

96 lines, 1 column(s) wide equals 96 equivalent lines at 0.395

cents per line.....\$37.92

Additional charges for notices containing rule or tabular work (50 per cent of above amount)

Charge for extra proofs of publication (\$1.00 for each proof in excess of two)

TOTAL AMOUNT OF CLAIM.....\$37.92

DATA FOR COMPUTING COST

Width of single column in picas 9.4

Size of type 8 point

Number of insertions 1

Pursuant to the provisions and penalties of IC 5-11-10-1, I hereby certify that the foregoing account is just and correct, that the amount claimed is legally due, after allowing all just credits, and that no part of the same has been paid.

I also certify that the printed matter attached hereto is a true copy, of the same column width and type size, which was duly published in said paper 1 times. The dates of publication being as follows:

03/18/20

Additionally, the statement checked below is true and correct

Newspaper does not have a Web site.

☒ Newspaper has a Web site and this public notice was posted on the same day as it was published in the newspaper.

Newspaper has a Web site, but due to technical problem or error, the public notice was posted on.....

Newspaper has Web site but refuses to post the public notice.

Date.....4-4-2020


 Title: Public Notice Clerk

Public Notice

Des. Nos. 1702957 & 190040

Monroe County is planning to undertake a roadway construction project, funded in part by the Federal Highway Administration. The project is located along Sunrise Greeting Court and West Vernal Pike and includes new alignment.

Under the preferred alternative, the proposed project would involve the extension of Sunrise Greeting Court south to Profile Parkway (which is currently under design), connecting West Vernal Pike to Profile Parkway and North Gates Drive via a railroad overpass. The project would include road and bridge construction on a new alignment over CSX Railroad (operated by the Indiana Railroad), with a total project length of approximately 0.5 miles. The typical roadway section will consist of two 12-foot lanes with 8-inch concrete curb and gutter, a 5-foot sidewalk on the west side and a 10-foot multiuse path on the east side of the proposed roadway, along with 3-foot bike lanes adjacent to the travel lanes. The anticipated project right-of-way needed for the Vernal Pike Connector is approximately 2.7 acres of permanent and 1.0 acres of temporary right-of-way. An additional approximately 1.82 acres of permanent right-of-way is required for the included segment of Profile Parkway.

The proposed action does not impact properties listed in or eligible for the National Register of Historic Places. The Indiana Department of Transportation (INDOT), on behalf of the FHWA, has issued a "No Historic Properties Affected" finding for this project due to the fact that no historic properties are present within the Area of Potential Effects (APE). In accordance with the National Historic Preservation Act, the views of the public are being sought regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e) and 800.6(a)(4). Pursuant to 36 CFR 800.4(d)(1), the documentation specified in 36 CFR 800.11(d) is available for inspection in DLZ Indiana, LLC's office located at 157 E. Maryland Street, Indianapolis, Indiana. Additionally, this documentation can be viewed electronically by accessing INDOT's Section 106 document posting website IN SCOPE at <http://erms.indot.in.gov/Section106Documents>.

This documentation serves as the basis for the "No Historic Properties Affected" finding. The views of the public on this effect finding are being sought. Please reply with any comments to Daniel J. Stevens, DLZ Indiana, LLC, 2211 E. Jefferson Boulevard, South Bend, IN 46615, Phone: 574-236-4400, Email: dstevens@dlz.com no later than April 18, 2020.

In accordance with the "Americans with Disabilities Act", if you have a disability for which Monroe County needs to provide accessibility to the document(s) such as interpreters or readers, please contact Lisa

Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



April 13, 2020

Dan Stevens
Environmental Scientist
DLZ Indiana, LLC
2211 East Jefferson Boulevard
South Bend, Indiana 46615

Federal Agency: Indiana Department of Transportation ("INDOT"),
on behalf of Federal Highway Administration, Indiana Division ("FHWA")

Re: Indiana Department of Transportation's finding of "no historic properties affect" on behalf of the
Federal Highway Administration for the proposed Vernal Pike Connector Project, in Bloomington
and Richland Townships, Monroe County, Indiana (Des. Nos. 1702957 and 1900406; DHPA No.
23905)

Dear Mr. Stevens:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the "Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO" or "INDNR-DHPA") has reviewed your March 13, 2020 review request submittal form, which enclosed the aforementioned finding, with 36 C.F.R. §800.11(d) documentation, which we received March 20, 2020.

As previously indicated, for the purposes of the Section 106 review of this federal undertaking, we agree that there are no historic properties listed in or eligible for inclusion in the National Register of Historic Places ("NRHP") within the area of potential effects.

Also as previously indicated, in terms of potential impact on archaeological resources, based on the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the original and additional portions of the proposed project area; and we concur with the opinion of the archaeologist, as expressed in the addendum Indiana archaeological short report (Bennett/Plunkett, 01/11/2019 & Plunkett, 11/18/2019), that no further archaeological investigations appear necessary.


If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery be reported to INDNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

Accordingly, we concur with INDOT's March 12, 2020, Section 106 finding of "No Historic Properties Affected" on behalf of FHWA for this federal undertaking.

The archaeological reviewer for this project on the Indiana SHPO staff is Wade T. Tharp, and the structures reviewer is Danielle Kauffmann. However, if you have questions about our comments or about a procedural issue, please contact initially an INDOT Cultural Resources staff member who is assigned to this project.

In any future correspondence regarding the proposed Vernal Pike Connector Project in Bloomington and Richland Townships, Monroe County, Indiana (Des. No. 1702957 and 1900406), please refer to DHPA No. 23905.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:DMK:dmk

emc: Erica Tait, FHWA
Anuradha Kumar, INDOT
Shaun Miller, INDOT
Susan Branigin, INDOT
Shirley Clark, INDOT
Dan Stevens, DLZ Indiana, LLC
Candace Hudziak, H&H Associates, LLC
Patrick Martin, Bloomington/Monroe County MPO
Danielle Bachant-Bell, Monroe County Historic Preservation Board of Review
Joshua Biggs, Indiana Landmarks, Central Regional Office
Wade T. Tharp, INDNR-DHPA
Danielle Kauffmann, INDNR-DHPA